



January 20, 2025

Deanna Schmidt, AICP
City Planner & Zoning Administrator
City of Fitchburg
5520 Lacy Road
Fitchburg, WI 53711

RE: McDonalds – Fitchburg, WI,
Project Narrative in support of the proposed Conditional Use Permit

Dear Deanna,

McDonald's is proposing to construct a new restaurant with parking lot and double drive at the above location. The proposed lot is part of an existing development known as Jamestown Quarry. McDonald's will be located near the Southwest corner of the overall development.

Access to the site will be internal from the proposed private drive which connects McKee and Fitchrona roads. McDonald's will have 35 on-site parking stalls to serve the restaurant. The double drive thru will be equipped with the latest digital menu boards, pre-browse boards, canopies, and vehicle height detector. The drive thru has stacking available for 18 full size vehicles. The proposed trash corral will be a 6' high, fully enclosed structure to match the building. The site and buildings will be designed to meet all ADA accessibility requirements including a pedestrian sidewalk from the McKee Road right of way to the restaurant. Utilities serving this site will be in close proximity to the lot both within the overall development and along the adjacent rights of way.

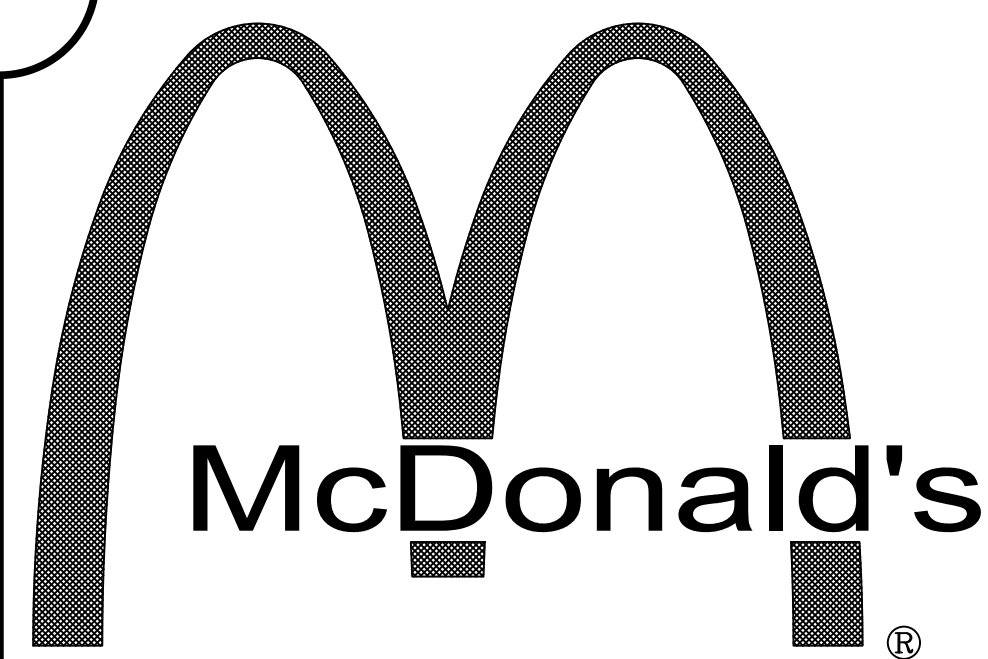
McDonald's is requesting a conditional use permit for the proposed menu boards as shown on the Final Engineering Plans provided. Please feel free to reach out with any questions or comments you may have. We look forward to working with the City on this project!

Respectfully,

A handwritten signature in black ink, appearing to read "Dan Olson".

Dan Olson, President
Upstream Design Group





FINAL ENGINEERING PLANS

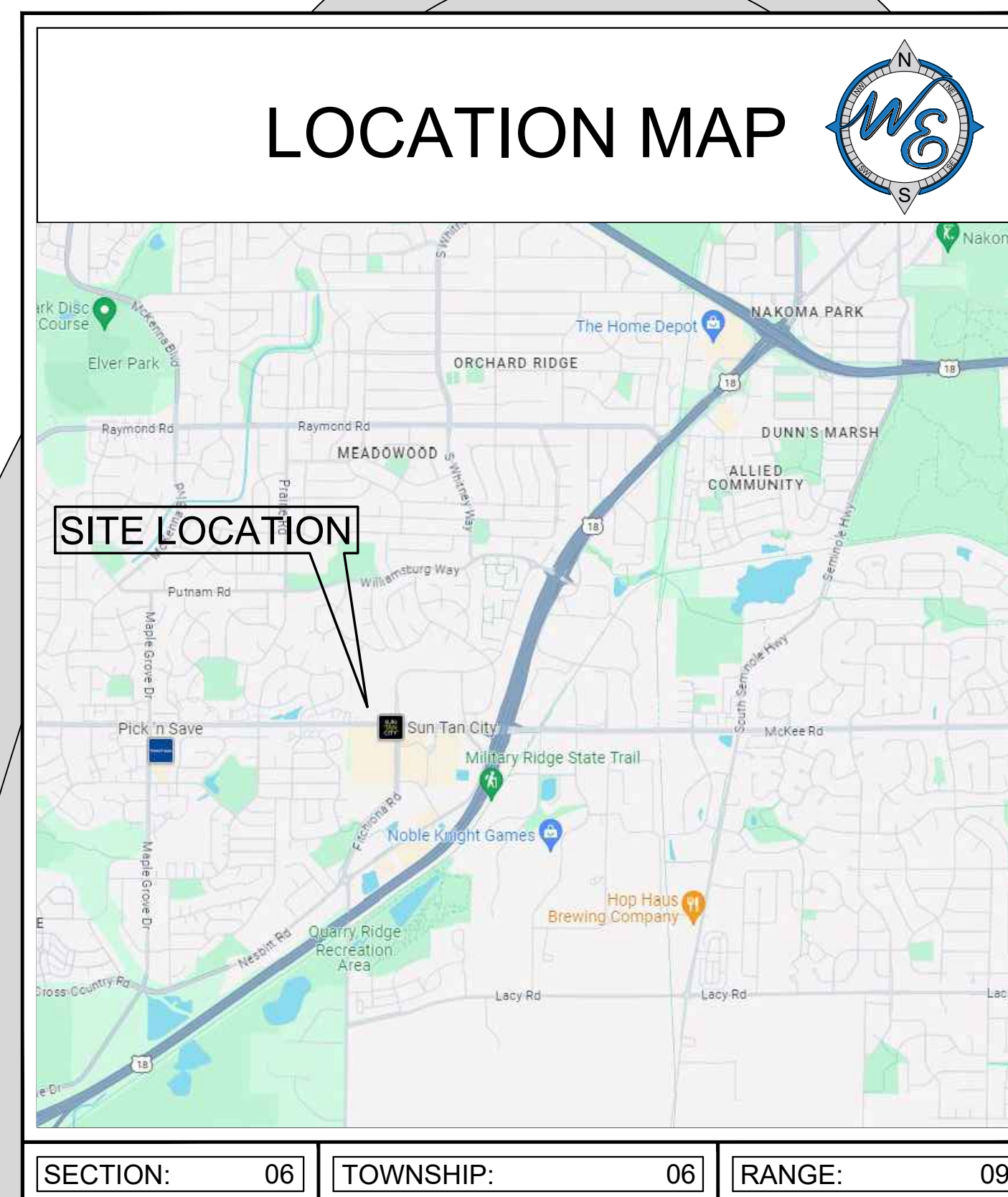
FOR

McDONALD'S - FITCHBURG, WI

AT
 NWC OF MCKEE ROAD AND FITCHRONA ROAD
 CITY OF FITCHBURG
 DANE COUNTY, WISCONSIN

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NOTE: THE DESIGNS CONTAINED IN THE ABOVE PLANS AND SUPPORTING DOCUMENTS WERE PREPARED WITH THE UNDERSTANDING THAT THEY WOULD BE USED AS A WHOLE PLAN SET. EACH CONSTRUCTION DISCIPLINE IS TO USE ALL THE PLANS AND SUPPORTING DOCUMENTS TOGETHER AS A WHOLE AND NOT AS SEPARATE DOCUMENTS. EACH CONTRACTOR IS TO BECOME COMPLETELY FAMILIAR WITH THE WHOLE PLAN SET AND THE EXISTING SITE CONDITIONS. SHOULD ANYTHING WITH ALL THESE PLANS AND SUPPORTING DOCUMENTS BE INCONSISTENT WITH THE SITE CONDITIONS THEN THE CONTRACTOR IS TO CONTACT THE ENGINEER IMMEDIATELY BEFORE ANY CONSTRUCTION IS STARTED.



EXISTING	DESCRIPTION	PROPOSED
	CATCH BASIN INLET	
	SANITARY MANHOLE	
	VALVE VAULT	
	FIRE HYDRANT	
	FLARED END SECTION	
	ELECTRICAL POWER POLE	
	OVERHEAD TRAFFIC SIGNAL	
	TRAFFIC SIGNAL MANHOLE	
	OVERHEAD ELECTRIC WIRES	
	TRANSFORMER PAD	
	TELEPHONE PEDESTAL	
	TELEPHONE MANHOLE	
	CABLE TELEVISION PEDESTAL	
	COMMONWEALTH EDISON MANHOLE	
	B/BOX	
	LIGHT POLE	
	SIGN	
	BOLLARD POLE	
	GAS MARKER	
	ELECTRIC MARKER	
	TELEPHONE MARKER	
	WATER MAIN	
	GAS MAIN	
	ELECTRIC LINE	
	TELEPHONE LINE	
	CABLE TV LINE	
	SANITARY SEWER	
	STORM SEWER	
	GUY POLE	
	CONIFEROUS TREE W/DIAMETER	
	DECIDUOUS TREE W/DIAMETER	
	WOOD FENCE	
	CHAIN LINK FENCE	
	METAL GUARDRAIL	
	CONCRETE SURFACE	
	DEPRESSED CURB	
	CONTOUR LINE	
	FINISHED FLOOR ELEVATION	
	PAVEMENT ELEVATION	
	MATCH EXISTING ELEVATION	
	GROUND ELEVATION	
	TOP OF WALK ELEVATION	
	TOP OF RETAINING WALL ELEVATION	
	FLOW LINE ELEVATION	
	TOP OF CURB ELEVATION	
	RIM ELEVATION	
	DOWNSPOUT LOCATION	
	PERVIOUS AREA SLOPE DIRECTION	
	PAVEMENT SLOPE DIRECTION	
	OVERLAND OVERFLOW DIRECTION	
	INLET PROTECTION	
	INLET BASKET FILTER	

BENCHMARK

REFERENCE BENCHMARK
 BEARINGS FOR THIS SURVEY AND MAP ARE REFERENCED TO THE WISCONSIN COUNTY COORDINATE SYSTEM, DANE COUNTY. THE SOUTH LINE OF THE SOUTHWEST QUARTER OF SECTION 06-06-09, RECORDED AS S88°12'32"E.
 ELEVATIONS ARE BASED ON THE NORTH AMERICAN VERTICAL DATUM OF 1988 (NAVD88). BENCHMARK IS A BRASS CAP IN CONCRETE MONUMENT MARKING THE SOUTHWEST CORNER OF SECTION 06, T06N R09E, ELEVATION = 1058.3'

SCOPE OF WORK

NEW CONSTRUCTION

Know what's Below.
 Call before you dig.



STATE OF ILLINOIS) SS
 COUNTY OF KANE)

I, JEFFREY C. MILLER, A LICENSED PROFESSIONAL ENGINEER OF WISCONSIN, HEREBY CERTIFY THAT THESE CIVIL ENGINEERING PLANS, NOT THE SUPPORTING DOCUMENTS, AS LISTED IN THE INDEX, HAVE BEEN PREPARED UNDER MY PERSONAL DIRECTION. THESE PLANS ARE INTENDED TO BE USED AS AN INTEGRAL PART OF THE PROJECT SPECIFICATIONS AND CONTRACT DOCUMENTS.

DATE: _____
 WISCONSIN LICENSED PROFESSIONAL ENGINEER NO. 33217-006.
 MY LICENSE EXPIRES ON 07-31-2026.
 UNLESS THIS DOCUMENT BEARS ORIGINAL SIGNATURE AND EMBOSSED SEAL OF THE DESIGN ENGINEER, IT IS NOT A VALID DOCUMENT.
 WISCONSIN PROFESSIONAL DESIGN FIRM LICENSE NO. 4258-11

COVER SHEET

DATE: 10/2/2025

NO. 1

REVISIONS

NO. 1

DATE: 10/2/2025

PREPARED FOR:

McDonald's
 110 N. Carpenter St.
 Chicago, IL 60607

McDONALD'S - FITCHBURG, WI
 NWC of McKee Road and Fitchrona Road
 Fitchburg, Wisconsin

Prepared By:

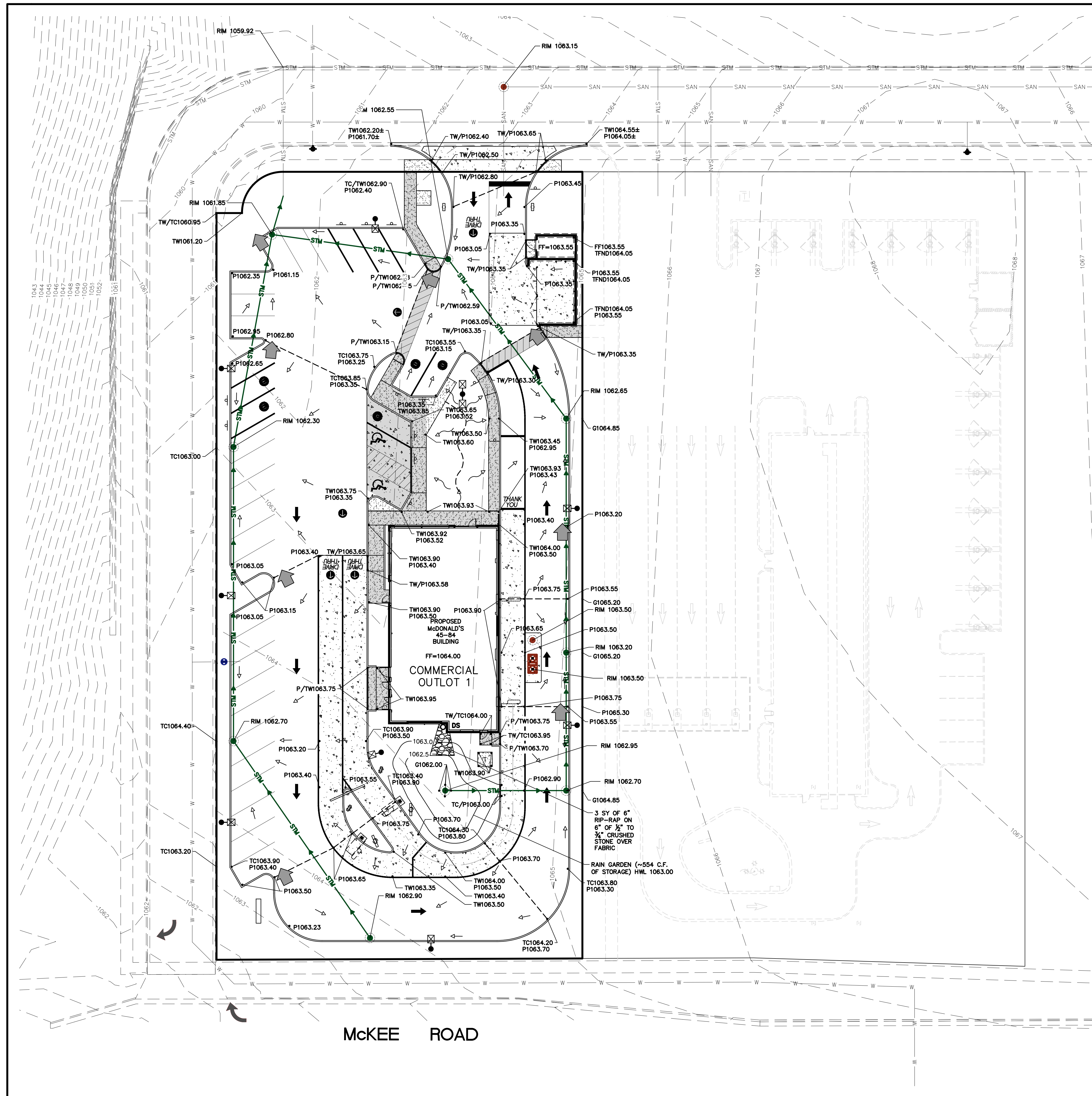
watermark-engineering.com | 2631 Ginger Woods Pkwy | Aurora, IL 60502 | (630) 975-1800

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CHECKED BY: J. MILLER
 DESIGN BY: J. VOLANTI
 DRAWN BY: J. VOLANTI
 DATE: DECEMBER 3, 2024
 SCALE: NONE
 PROJECT NO.: 24-001

C-1
 LC #48-1082

COVER SHEET



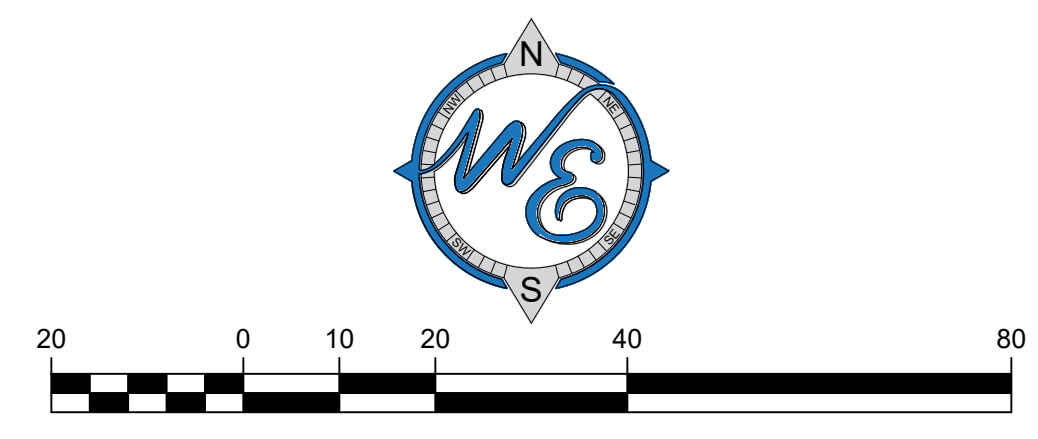
GENERAL NOTES:
 1. THESE PLANS ARE BASED ON THE FINAL ENGINEERING PLANS (PROJECT #22-11636 DATED 9/11/24) PREPARED BY: JSD 507 W. VERONA AVE., SUITE 500, VERONA, WI 53593
 2. PRIOR TO CONSTRUCTION, CONTRACTOR TO CONTACT THE DESIGN ENGINEER AND ARCHITECT TO VERIFY THAT THEY ARE WORKING FROM THE MOST CURRENT SET OF PLANS AND SPECIFICATIONS.

GRADING PLAN NOTES:
 1. UNLESS OTHERWISE SPECIFIED, TOP OF CURB (TC) AND/OR TOP OF WALK ELEVATIONS ARE 0.5' HIGHER THAN THE ADJACENT FLOW LINE (FL) OR PAVEMENT (P) ELEVATIONS.
 2. IN ALL LOCATIONS WHERE ELEVATIONS ARE SHOWN AS ±, THE ELEVATION HAS BEEN DETERMINED BASED ON INTERPOLATED GRADES FROM THE SURVEY. CONTRACTOR IS TO VERIFY THESE GRADES PRIOR TO CONSTRUCTION OF ANY IMPROVEMENTS WITHIN THE PROXIMITY OF THESE INTERPOLATED GRADES AND REPORT THEM TO THE DESIGN ENGINEER FOR VERIFICATION OF PROPOSED SLOPES PRIOR TO INSTALLATION OF PROPOSED IMPROVEMENTS. DESIGN ENGINEER IS NOT RESPONSIBLE FOR SLOPES OF PROPOSED IMPROVEMENTS BASED ON THESE ± GRADES WITHOUT CONFIRMATION OF EXISTING ELEVATIONS AT TIME OF CONSTRUCTION.
 3. PAVING, SIDEWALK, AND CURBING IS NOT TO BE INSTALLED IN SUCH A WAY THAT IT WILL BLOCK THE FLOW OF WATER AWAY FROM THE BUILDING INCLUDING BUT NOT LIMITED TO WEEP HOLES, WICKS, DRAINAGE SCUPPERS OR PIPES, AND LANDSCAPING.
 4. ALL RETAINING AND/OR DECORATIVE LANDSCAPE WALLS OR CURBS SHOWN ON THIS PLAN, INCLUDING DETAILS AND SECTIONS, ARE TO ILLUSTRATE GENERAL LOCATION, LENGTH, AND HEIGHT. STRUCTURAL DESIGN, INCLUDING PROPER DRAINAGE, TIE-BACKS, AND SHORING AS WELL AS CONSTRUCTION MEANS ARE NOT ADDRESSED AS PART OF THESE PLANS. A STRUCTURAL ENGINEER SHOULD BE ENGAGED BY THE GENERAL CONTRACTOR AS THEY DEEM NECESSARY. WATERMARK ENGINEERING RESOURCES ASSUMES NO LIABILITY FOR THE DESIGN OR CONSTRUCTION OF ANY STRUCTURAL ELEMENT.

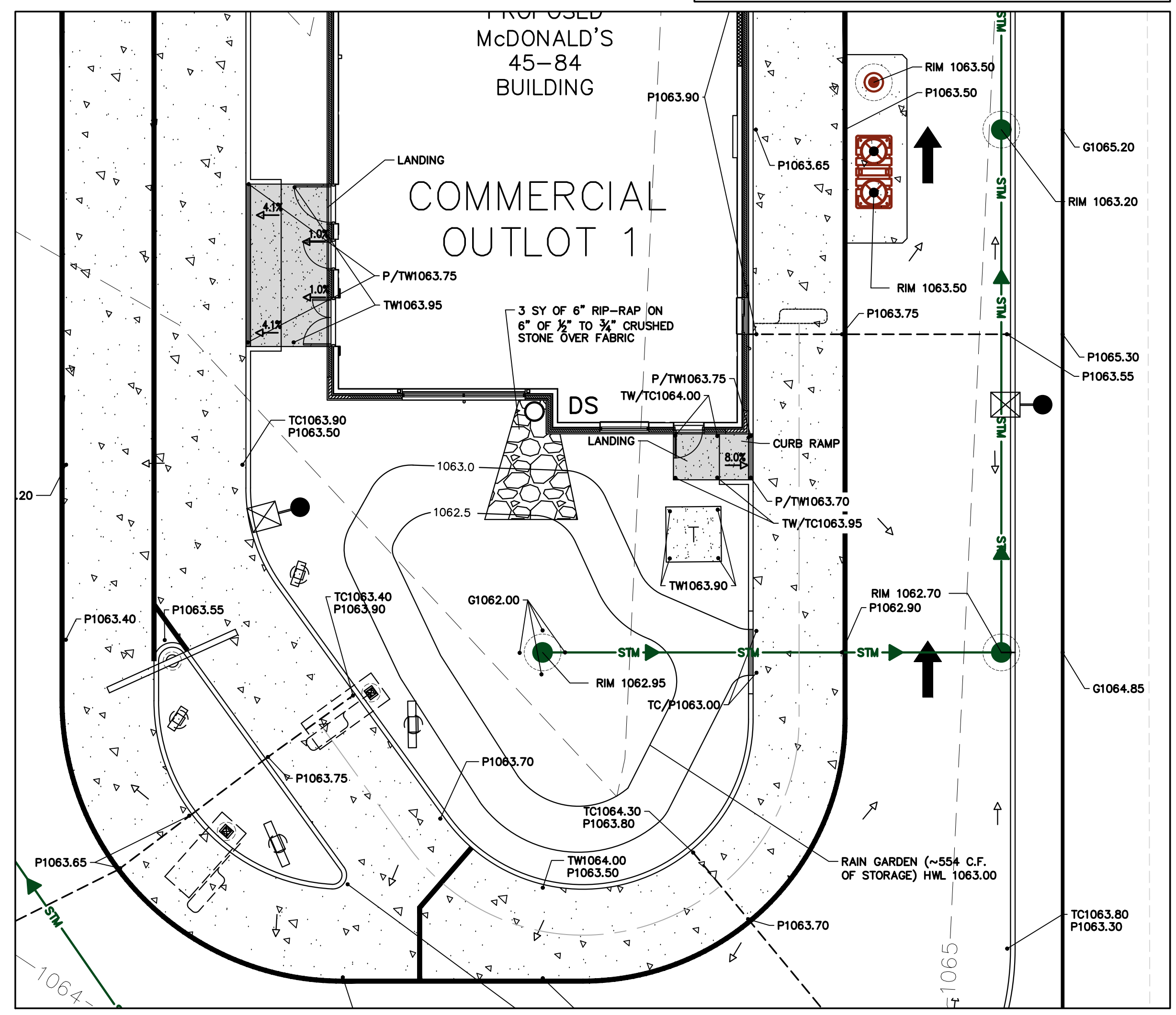
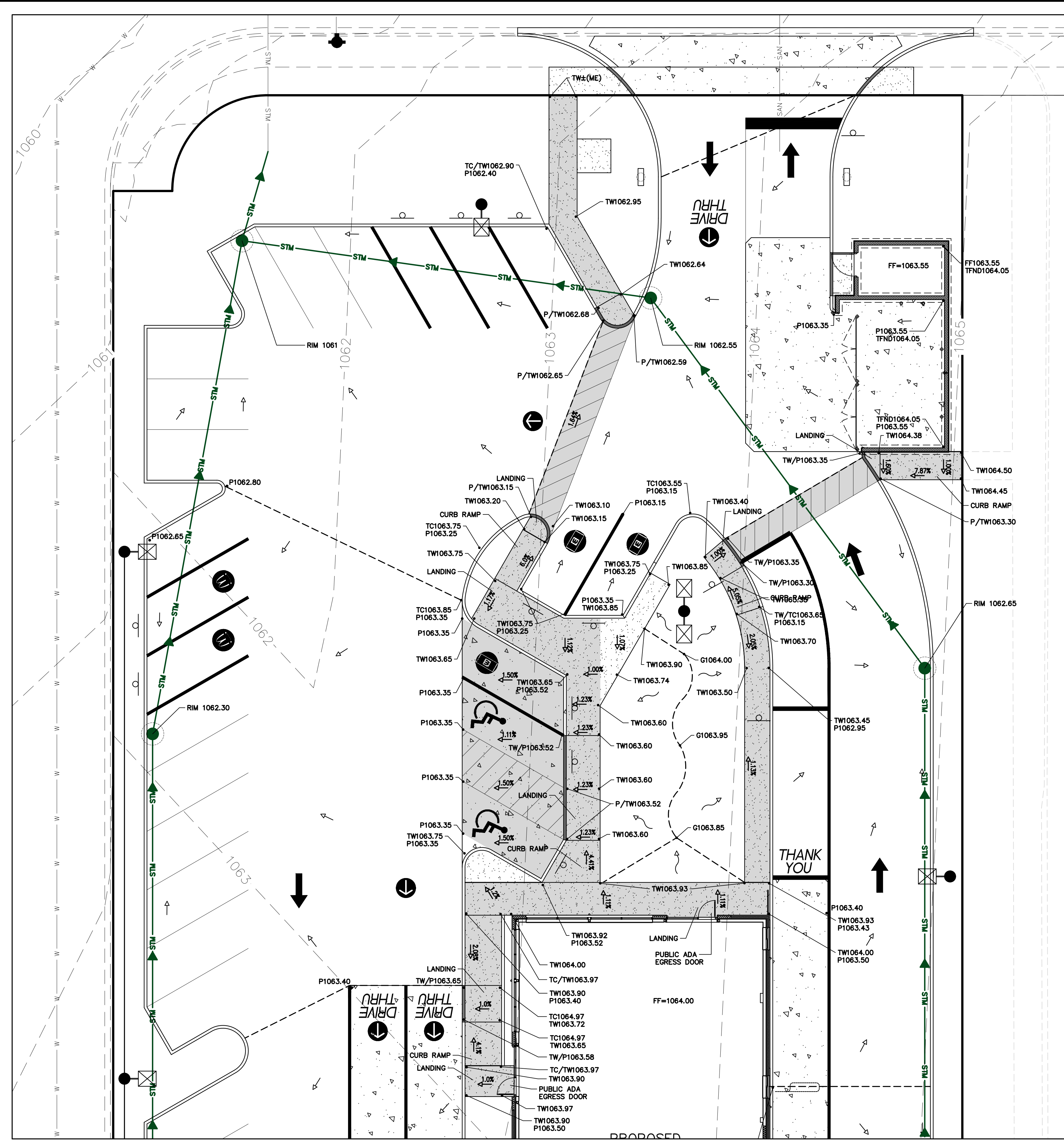
= A.D.A. ACCESSIBLE ROUTE (SEE SHEET C-4 FOR DETAILS)

DATE	10/20/25
REVISIONS	
NO.	1
DESCRIPTION	PER ARCHITECTURAL DESIGN COMMENTS & IN-HOUSE COORDINATION
Prepared For:	
McDonald's 110 N. Carpenter St. Chicago, IL 60607 McDONALD'S - FITCHBURG, WI NWC of McKee Road and Fitchrona Road Fitchburg, Wisconsin	
Prepared By:	

watermark-engineering.com | 2631 Ginger Woods Pkwy | Aurora, IL 60502 | (630) 375-1800
 PROJECT NO.: 24-001
 CHECKED BY: J. MILLER
 DESIGN BY: J. VOLANTI
 DRAWN BY: J. VOLANTI
 DATE: DECEMBER 3, 2024
 SCALE: 1" = 20'
 PROJECT NO.: 24-001



GRADING PLAN



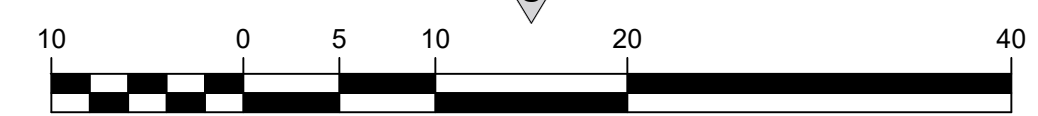
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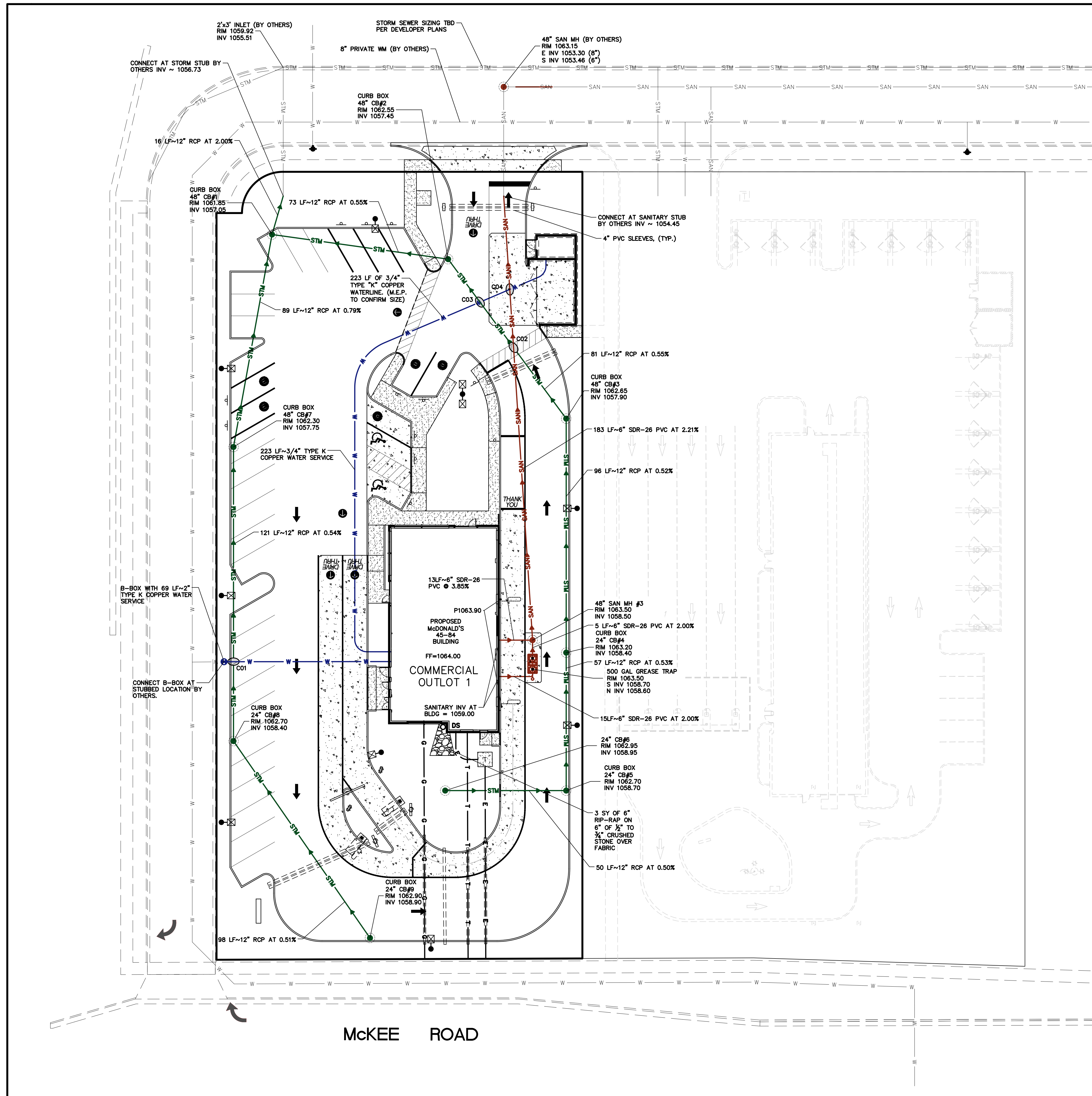
█ = A.D.A. ACCESSIBLE ROUTE



ACCESSIBLE ROUTE GRADES AND DETAILS

DATE: 10/2/2025	REVISIONS:	NO. 1	DESCRIPTION: PER ARCHITECTURAL DESIGN COMMENTS & IN-HOUSE COORDINATION
Prepared For:			
McDonald's 110 N. Carpenter St. Chicago, IL 60607 McDONALD'S - FITCHBURG, WI NWC of McKee Road and Fitchrona Road Fitchburg, Wisconsin			
Prepared By:			
watermark-engineering.com 2631 Ginger Woods Pkwy Aurora, IL 60502 (630) 375-1800			
CHECKED BY: J. MILLER	DESIGN BY: J. VOLANTI	DRAWN BY: J. VOLANTI	DATE: DECEMBER 3, 2024
PROJECT NO.: 24-001		SCALE: 1" = 10'	
C-4			LC #48-1082

ACCESSIBLE ROUTE GRADES AND DETAILS



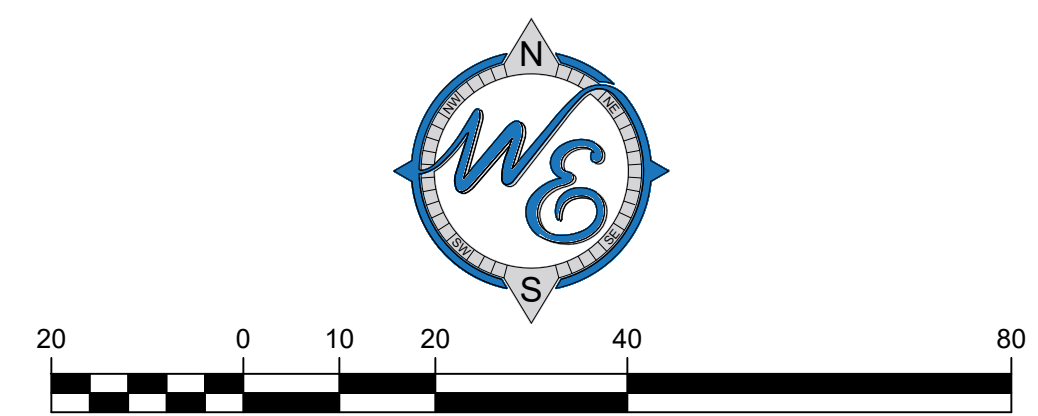
GENERAL NOTES:
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 PREPARED BY: JSD
 507 W. VERONA AVE., SUITE 500, VERONA, WI 53593

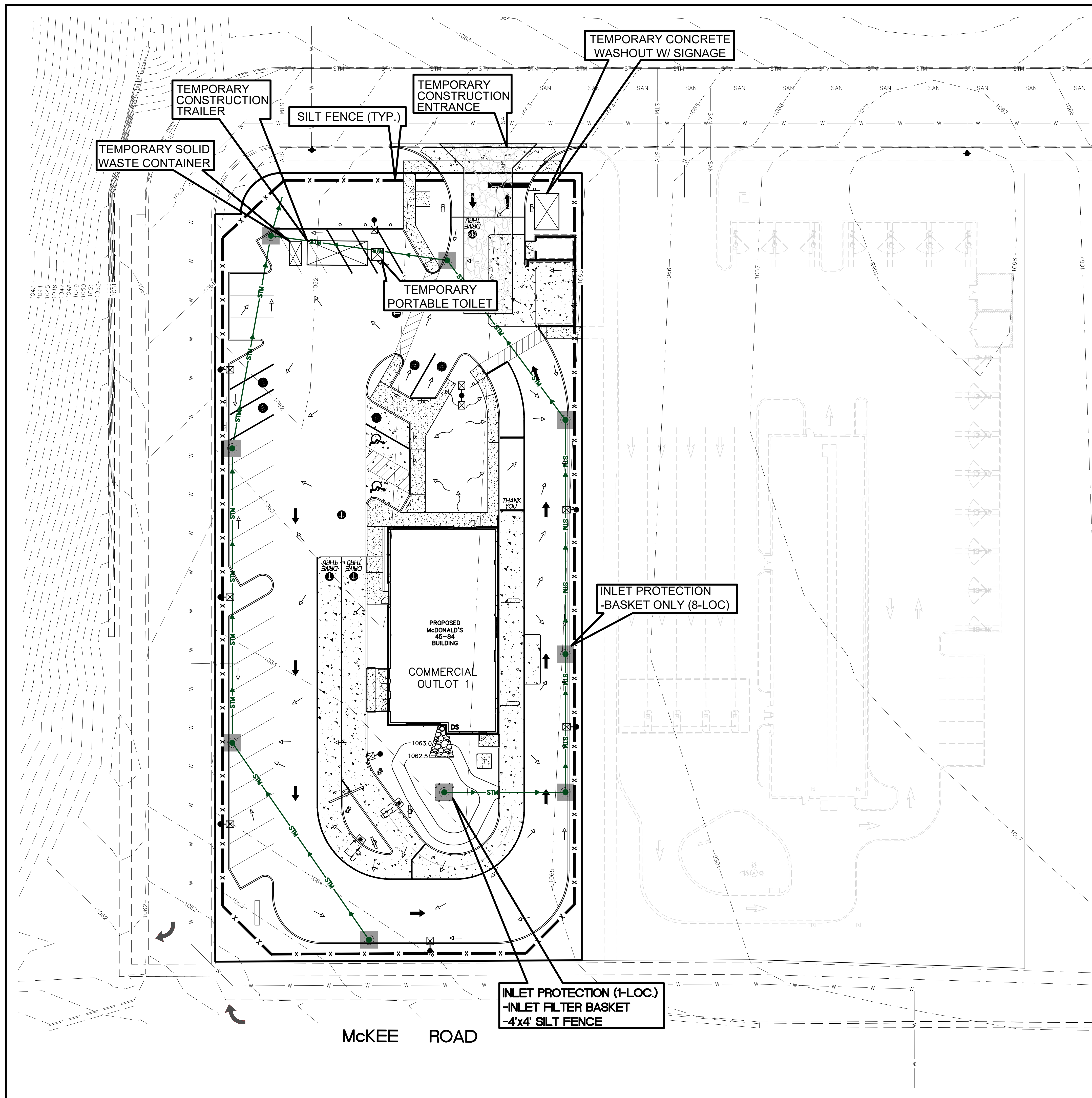
UTILITY PLAN NOTES:
 1. PRIOR TO CONSTRUCTION OF ANY UTILITIES, CONTRACTOR IS TO VERIFY THAT THE PROPOSED UTILITIES SHOWN ON THIS PLAN THAT ENTER THE PROPOSED BUILDING(S) CORRESPOND WITH THE UTILITIES ON THE PLUMBING PLANS AS THEY EXIT THE BUILDING(S). CONTRACTOR TO REPORT IN WRITING ANY DISCREPANCIES IN SIZE, LOCATION, OR INVERT ELEVATION TO THE DESIGN ENGINEER IMMEDIATELY FOR RESOLUTION OF THE CONFLICT IN WRITING.
 2. GENERAL CONTRACTOR TO COORDINATE THE INSTALLATION AND PERMITTING OF THE PUBLIC UTILITIES, SUCH AS GAS, ELECTRIC, TELEPHONE, CABLE AND FIBER OPTICS, WITH THE PUBLIC UTILITY COMPANIES AND ARCHITECT PRIOR TO CONSTRUCTION. THE INSTALLATION OF THE PUBLIC UTILITIES AND NECESSARY SLEEVING TO BE INCLUDED AS PART OF GENERAL CONTRACTOR'S SCOPE OF WORK FOR THIS PROJECT.

UTILITY CROSSINGS

CO1	BOTTOM OF PROPOSED 12" STORM SEWER = 1058.22'	TOP OF PROPOSED 2" WATER SERVICE = 1056.50'	> 1.73'
CO2	BOTTOM OF PROPOSED 12" STORM SEWER = 1057.74'	TOP OF PROPOSED 6" SANITARY SERVICE = 1056.46'	> 1.28'
CO3	BOTTOM OF PROPOSED 12" STORM SEWER = 1057.57'	TOP OF PROPOSED 3/4" WATER SERVICE = 1056.00'	> 1.57'
CO4	BOTTOM OF PROPOSED 3/4" WATER SERVICE = 1057.55'	TOP OF PROPOSED 6" SANITARY SERVICE = 1055.80'	> 1.75'

DATE: 10/9/2025	NO. 1	REVISIONS: PER ARCHITECTURAL DESIGN COMMENTS & IN-HOUSE COORDINATION
Prepared For:		
McDonald's 110 N. Carpenter St. Chicago, IL 60607 McDONALD'S - FITCHBURG, WI NWC of McKee Road and Fitchrona Road Fitchburg, Wisconsin		
Prepared By:		
watermark-engineering.com 2631 Ginger Woods Pkwy Aurora, IL 60502 (630) 375-1800		
CHECKED BY: J. MILLER	DESIGN BY: J. VOLANTI	DRAWN BY: J. VOLANTI
DATE: DECEMBER 3, 2024	SCALE: 1" = 20'	PROJECT NO.: 24-001
UTILITY PLAN		C-5
UTILITY PLAN		LC #48-1082





LEGEND

	SILT FENCE
	INLET FILTER BASKET
	INLET PROTECTION

GENERAL NOTES:

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DATE	10/20/25
NO.	1
REVISIONS	NO REVISIONS

Prepared For:

McDonald's
 110 N. Carpenter St.
 Chicago, IL 60607

McDONALD'S - FITCHBURG, WI
 NWC of McKee Road and Fitchrona Road
 Fitchburg, Wisconsin

Prepared By:

Watermark Engineering Resources
 watermark-engineering.com | 2631 Ginger Woods Pkwy | Aurora, IL 60502 | (630) 375-1800

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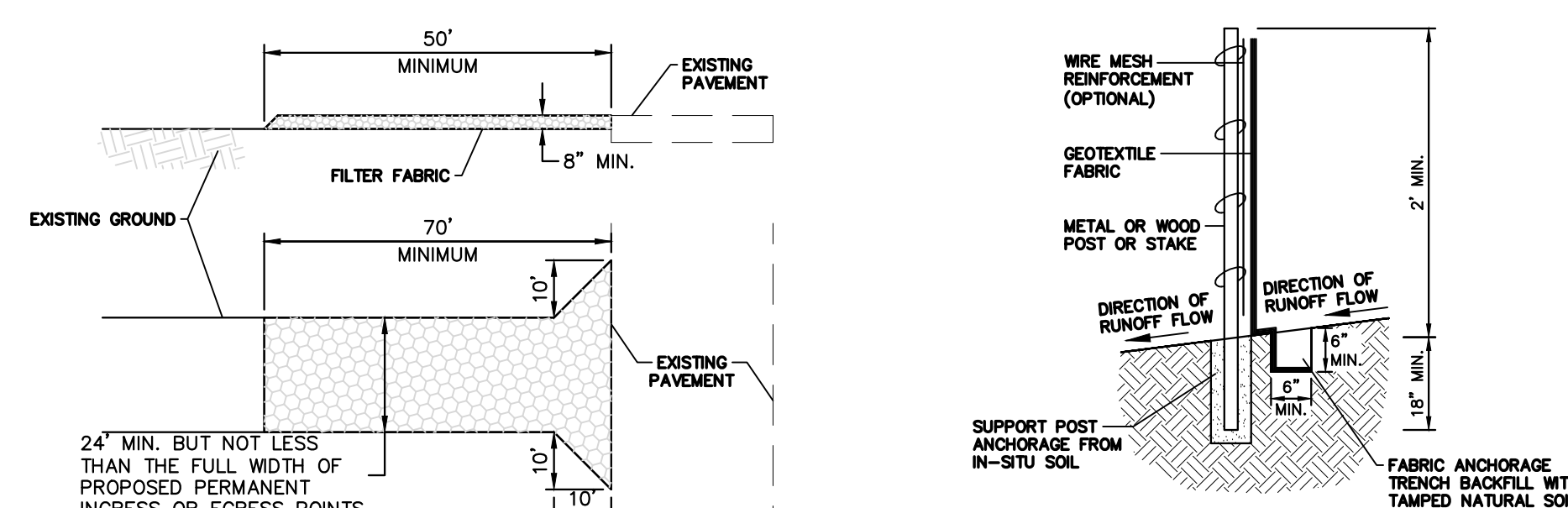
PHASE II SOIL EROSION CONTROL PLAN

C-7
 LC #48-1082

PHASE II SOIL EROSION CONTROL PLAN

EROSION CONTROL

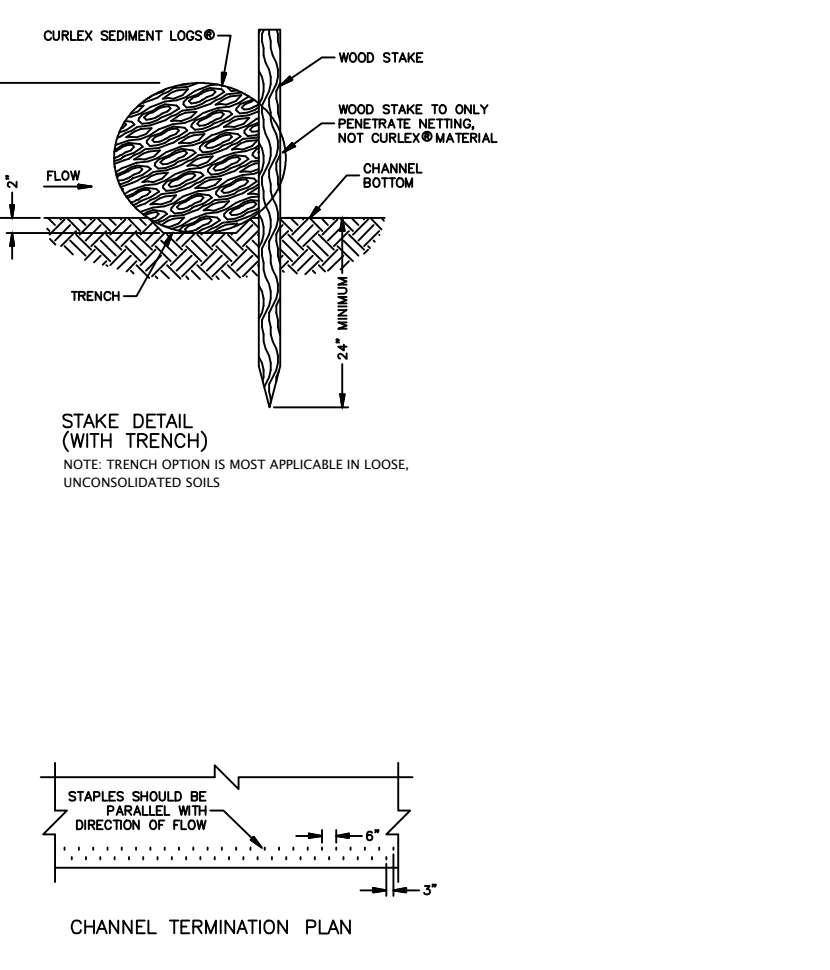
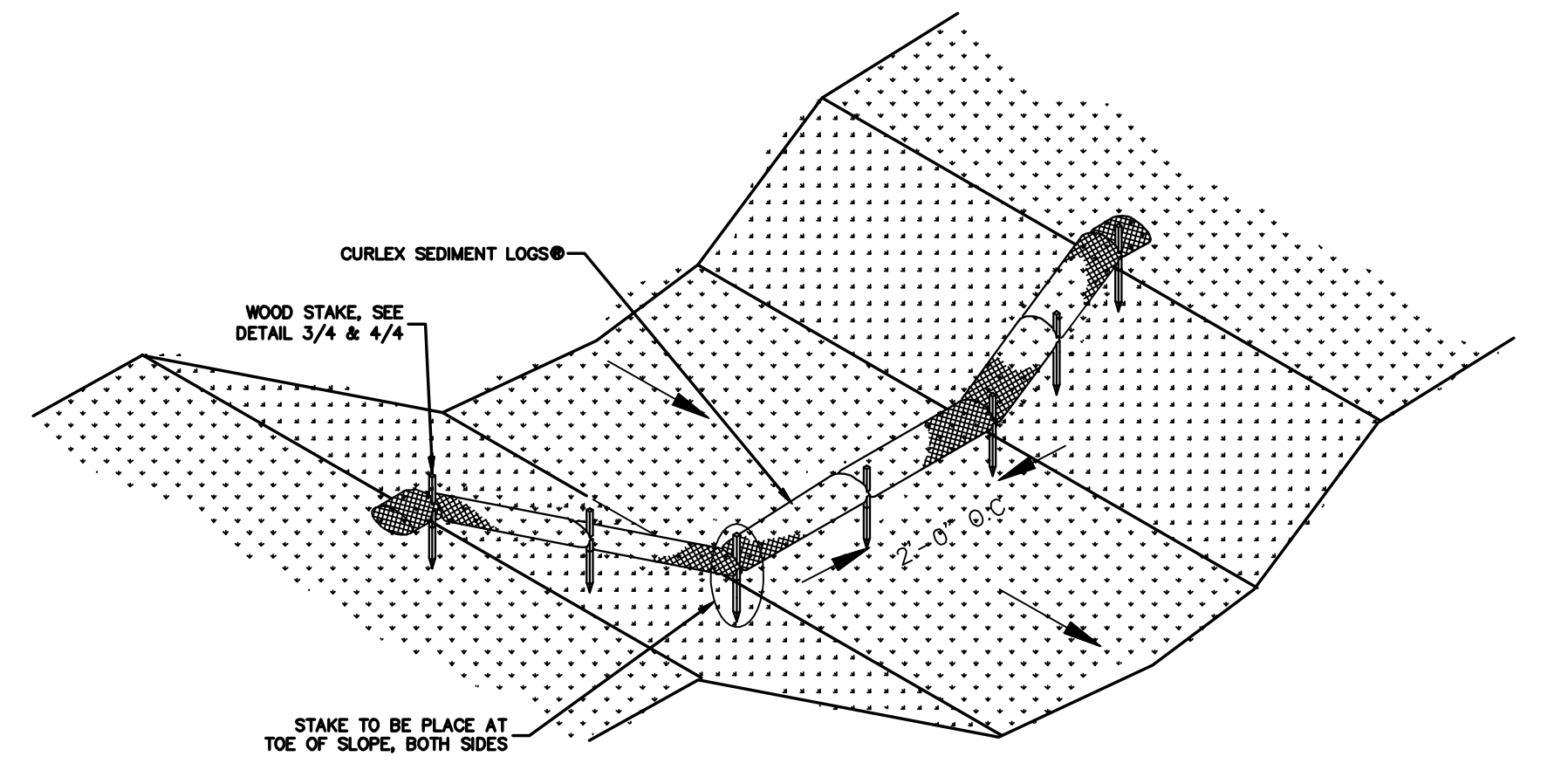
- CONTRACTOR IS TO FOLLOW THE REQUIREMENTS OF THE "CHAPTER NR 216 WISCONSIN ADMINISTRATIVE CODE" CURRENT EDITION AND THE REQUIREMENTS OF THE NATIONAL POLLUTION DISCHARGE ELIMINATION SYSTEM (NPDES).
- SOIL EROSION CONTROL SYSTEMS SHALL BE CONSTRUCTED AS SHOWN ON THE SOIL EROSION CONTROL PLANS AND/OR AS SPECIFIED BY THE DESIGN ENGINEER, VILLAGE ENGINEER, APPOINTED SWPPP INSPECTOR, OR MUNICIPAL INSPECTOR.
- PERIMETER EROSION BARRIER SHALL BE PLACED IN A MANNER THAT WILL INTERCEPT WATER BORNE SILT AND PREVENT IT FROM LEAVING THE AREA OF CONSTRUCTION. ALL SILT FENCES SHALL BE PLACED AS CLOSE TO THE CONTOUR AS POSSIBLE WITH THE ENDS EXTENDING UPSLOPE. THE MAXIMUM SPACING OF POSTS SHALL BE 5 FEET. WHEN WIRE OR OTHER FORM OF APPROVED BACKING IS USED THE MAXIMUM SPACING MAY BE INCREASED TO 8 FEET. SPACING MAY NEED TO BE ADJUSTED SO THAT POSTS ARE LOCATED IN LOW AREAS WHERE WATER MAY POND. THE FILTER FABRIC AND WIRE SUPPORT, IF USED, MUST BE SECURELY FASTENED TO THE UPSLOPE SIDE OF THE POSTS USING HEAVY DUTY WIRE STAPLES AT LEAST ONE INCH LONG OR TIE WIRES (10 GAGE MINIMUM). THE FABRIC SHALL NOT BE STAPLED OR WIRED TO THE WIRE SUPPORT OR TO EXISTING TREES. GEOTEXTILE FABRIC SHALL MEET THE REQUIREMENTS OF MATERIAL SPECIFICATION 592 (GEOTEXTILE) TABLE 1 OR 2, CLASS 1 IN THE ILLINOIS URBAN MANUAL, CURRENT EDITION. THE FABRIC SHALL HAVE AN AOS OF AT LEAST 30 FOR NONWOVEN AND 50 FOR WOVEN MATERIAL.
- INLET FILTERS SHALL BE CONSTRUCTED OF A REPLACEABLE REINFORCED FILTER BAG SUSPENDED FROM A RETAINER RING OR FRAME. INLET FILTER SYSTEMS SHALL BE THE CATCH-ALL WITH OVERFLOW, AS FURNISHED BY MARATHON MATERIALS INC., OR PRE-APPROVED EQUAL. CARE SHOULD BE TAKEN WHEN MAINTAINING OR REMOVING THIS FILTER FABRIC BAG TO NOT ALLOW THE PREVIOUSLY TRAPPED DEBRIS TO ENTER THE STORM SEWER SYSTEM.
- THE BED FOR RIP RAP SHALL BE TRIMMED AND SHAPED TO ALLOW THE FINISHED SURFACE TO CONFORM TO THE LINES SPECIFIED. AT THE TOE OF THE SLOPE, THE RIP RAP SHALL COMMENCE ON A CONTINUATION OF THE SLOPE. EXCAVATION TO ACCOMMODATE THE FULL DEPTH OF FABRIC, BEDDING LAYER, AND RIP RAP SPECIFIED.
- FILTER FABRIC IS REQUIRED UNDER STONE RIP RAP GRADATION 4, 5, 6 AND 7 FOR ALL USES, AND UNDER CONCRETE BLOCK, BROKEN CONCRETE, AND STONE OR BROKEN CONCRETE DUMPED RIP RAP WHEN USED FOR SOIL EROSION PROTECTION.
- STREETS ARE TO BE CLEARED OF DEBRIS, AND SWEEP CLEAN OF SILT AND MUD DAILY.
- SOIL EROSION CONTROL MEASURES ARE TO BE CHECKED BY QUALIFIED PERSONNEL AT LEAST ONCE EVERY 7 CALENDAR DAYS AND WITHIN 24 HOURS OF THE END OF A STORM THAT IS 0.5 INCHES OR GREATER OR EQUIVALENT SNOWFALL AND REPAIRED IF NECESSARY.
- ALL EROSION CONTROL PROTECTION SHALL BE KEPT IN PLACE UNTIL THE GROUND HAS BEEN STABILIZED AND THE PAVEMENT HAS BEEN INSTALLED.
- ANY DISTURBED AREAS ON WHICH CONSTRUCTION ACTIVITIES HAVE STOPPED (PERMANENTLY OR TEMPORARILY) FOR 7 DAYS, MUST BE STABILIZED IN ACCORDANCE WITH NPDES REQUIREMENTS.
- BUILT UP SEDIMENT SHALL BE REMOVED FROM THE SILT FENCE WHEN IT HAS REACHED ONE-THIRD THE HEIGHT OF THE FENCE.
- SILT FENCES SHALL BE INSPECTED FOR DEPTH OF SEDIMENT, TEARS, ETC., TO SEE IF FABRIC IS SECURELY ATTACHED TO THE FENCE POSTS, AND THAT THE FENCE POSTS ARE SECURELY IN THE GROUND.
- THE SEDIMENT BASIN, IF PRESENT, SHALL BE INSPECTED FOR DEPTH OF SEDIMENT AT LEAST ONCE A WEEK. BUILD UP SEDIMENT SHALL BE REMOVED WHEN IT REACHES 25 PERCENT OF THE DESIGN CAPACITY.
- CONTRACTOR TO COMPLY WITH FINAL STABILIZATION AND TERMINATION REQUIREMENTS OF THE SWPPP.
- AT A MINIMUM, SILT FENCE AND OTHER EROSION CONTROL MEASURES SHALL BE INSTALLED IN THE LOCATIONS SHOWN ON THE PHASE I AND PHASE II SOIL EROSION CONTROL PLANS. THEY SHALL ALSO BE INSTALLED ANYWHERE THAT THEY ARE NEEDED DURING CONSTRUCTION IN ORDER TO PREVENT EROSION AND SEDIMENT FROM BEING CARRIED DOWN STREAM. THIS IS THE GENERAL CONTRACTOR'S RESPONSIBILITY AND SHALL BE INSTALLED, RELOCATED, MAINTAINED, ETC. AS DIRECTED BY THE APPOINTED SWPPP INSPECTOR. EROSION CONTROL INSTALLATION AND MAINTENANCE IS TO BE A PART OF THE CONTRACT AND IS NOT AN EXTRA TO THE OWNER.



- NOTES:**
- STONE SIZE - 3" ROCK.
 - THICKNESS - NOT LESS THAN TWELVE (12) INCHES.
 - FILTER FABRIC - SHALL BE PLACED OVER THE ENTIRE AREA PRIOR TO PLACING OF STONE. GEOTEXTILE FABRIC SHALL MEET THE REQUIREMENTS OF MATERIAL CLASS II, III OR IV IN THE WISCONSIN URBAN MANUAL.
 - STONE PLACEMENT - THE STONES IN THE ENTRANCE SHALL BE PLACED ACCORDING TO WISCONSIN DEPARTMENT OF NATURAL RESOURCES TECHNICAL STANDARD 1057.
 - MAINTENANCE - THE ENTRANCE SHALL BE MAINTAINED IN A CONDITION WHICH WILL PREVENT TRACKING OR FLOWING OF SEDIMENT ONTO PUBLIC RIGHT-OF-WAY. THIS MAY REQUIRE PERIODIC TOP DRESSING WITH ADDITIONAL STONE AS CONDITIONS DEMAND AND REPAIR AND/OR CLEANOUT OF ANY MEASURES USED TO TRAP SEDIMENT. ALL SEDIMENT SPILLED, DROPPED, WASHED OR TRACKED ONTO PUBLIC RIGHT-OF-WAY MUST BE REMOVED IMMEDIATELY.
 - WASHING - WHEELS SHALL BE CLEANED TO REMOVE SEDIMENT PRIOR TO ENTRANCE ONTO PUBLIC RIGHT-OF-WAY. WHEN WASHING IS REQUIRED, IT SHALL BE DONE ON AN AREA STABILIZED WITH STONE AND WHICH DRAINS INTO AN APPROVED SEDIMENT TRAPPING DEVICE. ALL SEDIMENT SHALL BE PREVENTED FROM ENTERING STORM DRAINS, DITCHES, WATERCOURSES, OR SURFACE WATERS INCLUDING WETLANDS.
 - PERIODIC INSPECTION AND NEEDED MAINTENANCE SHALL BE PROVIDED AFTER EACH RAIN.

STABILIZED CONSTRUCTION ENTRANCE

SILT FENCE DETAIL



SEDIMENT LOG DITCH CHECK

NO SCALE

SOIL PROTECTION CHART

	JAN	FEB	MAR	APR	MAY	JUNE	JULY	AUG	SEP	OCT	NOV	DEC
PERMANENT SEEDING												
SODDING												
TEMPORARY SEEDING												
MULCHING												

- NOTES:**
- PERMANENT VEGETATION SHALL BE PLANTED ACCORDING TO THE APPROVED LANDSCAPE PLAN AND SHALL FOLLOW ILLINOIS URBAN MANUAL PRACTICE STANDARD 880 FOR PERMANENT SEEDING AND 925 FOR SODDING AT A MINIMUM.
 - TEMPORARY SEEDING SHALL BE APPLIED ACCORDING TO WISCONSIN DEPARTMENT OF NATURAL RESOURCES TECHNICAL STANDARD 1059. THIS PRACTICE APPLIES TO ALL CLEARED, UNVEGETATED, OR SPARSELY VEGETATED SOIL SURFACES WHERE VEGETATIVE COVER IS NEEDED FOR LESS THAN 1 YEAR.
 - WHERE THE PH OF THE SOIL IS BELOW 5.5, APPLY ONE AND ONE HALF TO TWO TONS PER ACRE OF FINELY GROUND AGRICULTURAL LIMESTONE. IF THE SEEDING PERIOD IS LESS THAN 30 DAYS, LIMING WILL NOT BE REQUIRED.
 - APPLY 500 POUNDS PER ACRE OF 10-10-10 FERTILIZER OR EQUIVALENT. INCORPORATE LIME AND FERTILIZER INTO THE TOP 2-4 INCHES OF SOIL. IF THE SEEDING PERIOD IS LESS THAN 30 DAYS, FERTILIZER IS NOT REQUIRED.
 - PREPARE A TOPSOIL SEEDBED OF LOOSE SOIL TO A DEPTH OF 3 TO 4 INCHES. IF RECENT TILLAGE OR GRADING OPERATIONS HAVE RESULTED IN A LOOSE SURFACE, ADDITIONAL TILLAGE OR ROUGHENING MAY NOT BE REQUIRED EXCEPT TO BREAK UP LARGE CLODS. IF RAINFALL CAUSED THE SURFACE TO BECOME SEALED OR CRUSTED, LOOSEN IT JUST PRIOR TO SEEDING BY DISKING, RAKING, HARROWING, OR OTHER SUITABLE METHODS. GROVE OR FURROW SLOPES STEEPER THAN 3:1 ON THE CONTOUR BEFORE SEEDING.
 - SEED SHALL BE EVENLY APPLIED WITH A CYCLONE SEEDER, DRILL, CULTIPACKER SEEDER OR HYDROSEEDER. SMALL GRAINS SHALL BE PLANTED NO MORE THAN ONE INCH DEEP. GRASSES SHALL BE PLANTED NO MORE THAN ONE HALF INCH DEEP.
 - COVER BROADCAST SEEDINGS BY CULTIPACKING, DRAGGING A HARROW, OR RAKING.
 - GRASSES SHALL BE APPLIED AT 90 LBS PER ACRE AND SHALL ONLY BE APPLIED EARLY SPRING TO JULY 1.
 - CEREAL RYE SHALL BE APPLIED AT 90 LBS PER ACRE AND SHALL ONLY BE APPLIED EARLY SPRING TO SEPTEMBER 30.
 - WHEAT SHALL BE APPLIED AT 90 LBS PER ACRE AND SHALL ONLY BE APPLIED EARLY SPRING TO SEPTEMBER 30.
 - PERENNIAL RYE GRASS SHALL BE APPLIED AT 25 LBS PER ACRE AND SHALL ONLY BE APPLIED EARLY SPRING TO SEPTEMBER 30.
 - TEMPORARY MULCHES ARE TO BE APPLIED TO:
 - AREAS THAT HAVE BEEN SEEDED TO PROVIDE A TEMPORARY OR PERMANENT SEEDING;
 - AREAS THAT CANNOT BE SEEDED BECAUSE OF THE SEASON OF THE YEAR AND NEED FOR SOIL SURFACE PROTECTION;
 - FOR MUD AND DUST CONTROL;
 - PROVIDE PROTECTION DURING PERIODS WHEN CONSTRUCTION OR SEEDING CANNOT BE DONE.

PHASE I CONSTRUCTION SEQUENCE:

- INSTALL STABILIZED CONSTRUCTION ENTRANCE.
- PREPARE TEMPORARY PARKING AND STORAGE AREA.
- CONSTRUCT THE SILT FENCES ON THE SITE.
- INSTALL INLET PROTECTION AROUND ALL EXISTING STORM SEWER STRUCTURES.
- CONSTRUCT THE SEDIMENTATION BASINS.
- CONSTRUCT DIVERSION DITCHES AND AGGREGATE DITCH CHECKS TO DIRECT WATER TO THE SEDIMENTATION BASINS.
- HOLD PRE-CONSTRUCTION MEETING TO DISCUSS THE STORM WATER POLLUTION PLAN WITH ENGINEER, ALL CONTRACTORS AND JURISDICTIONAL INSPECTION AGENCIES.
- DEMOLISH THE SITE.
- START CONSTRUCTION OF BUILDING PAD AND STRUCTURES.
- BEGIN MASS GRADING OPERATIONS FOR THE SITE.

PHASE II CONSTRUCTION SEQUENCE:

- TEMPORARILY SEED DENUDED AREAS.
- INSTALL UTILITIES, UNDERDRAINS, STORM SEWERS, CURBS AND GUTTERS.
- INSTALL RIP RAP AROUND OUTLET STRUCTURES.
- INSTALL INLET PROTECTION AROUND ALL STORM SEWER STRUCTURES.
- PREPARE SITE FOR PAVING.
- PAVE SITE.
- INSTALL INLET PROTECTION DEVICES.
- COMPLETE GRADING AND INSTALL PERMANENT SEEDING AND PLANTING.
- REMOVE ALL TEMPORARY EROSION AND SEDIMENT CONTROL DEVICES (ONLY IF SITE IS STABILIZED).

FIGURE 1. INLET PROTECTION TYPES A, B AND C

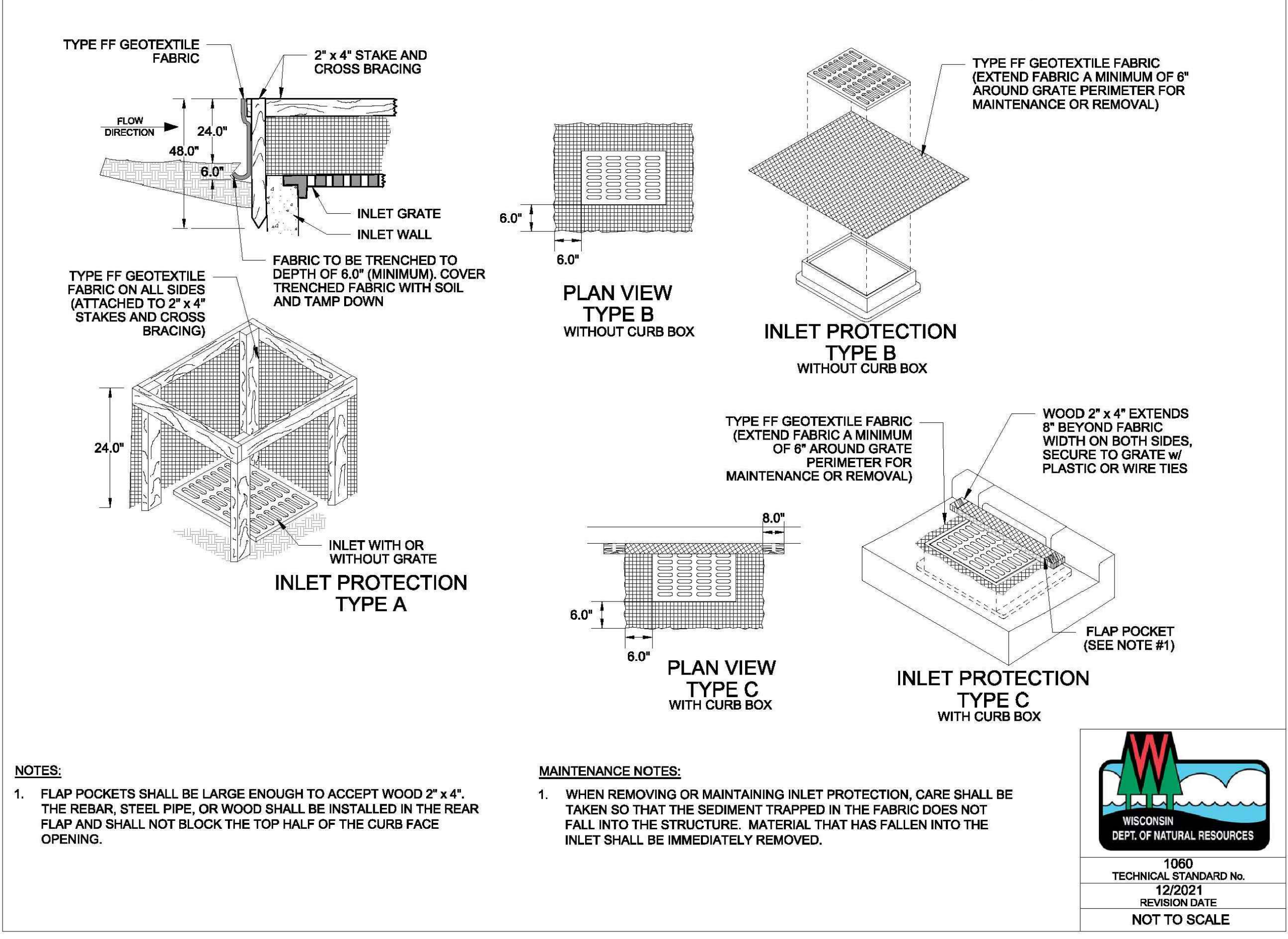
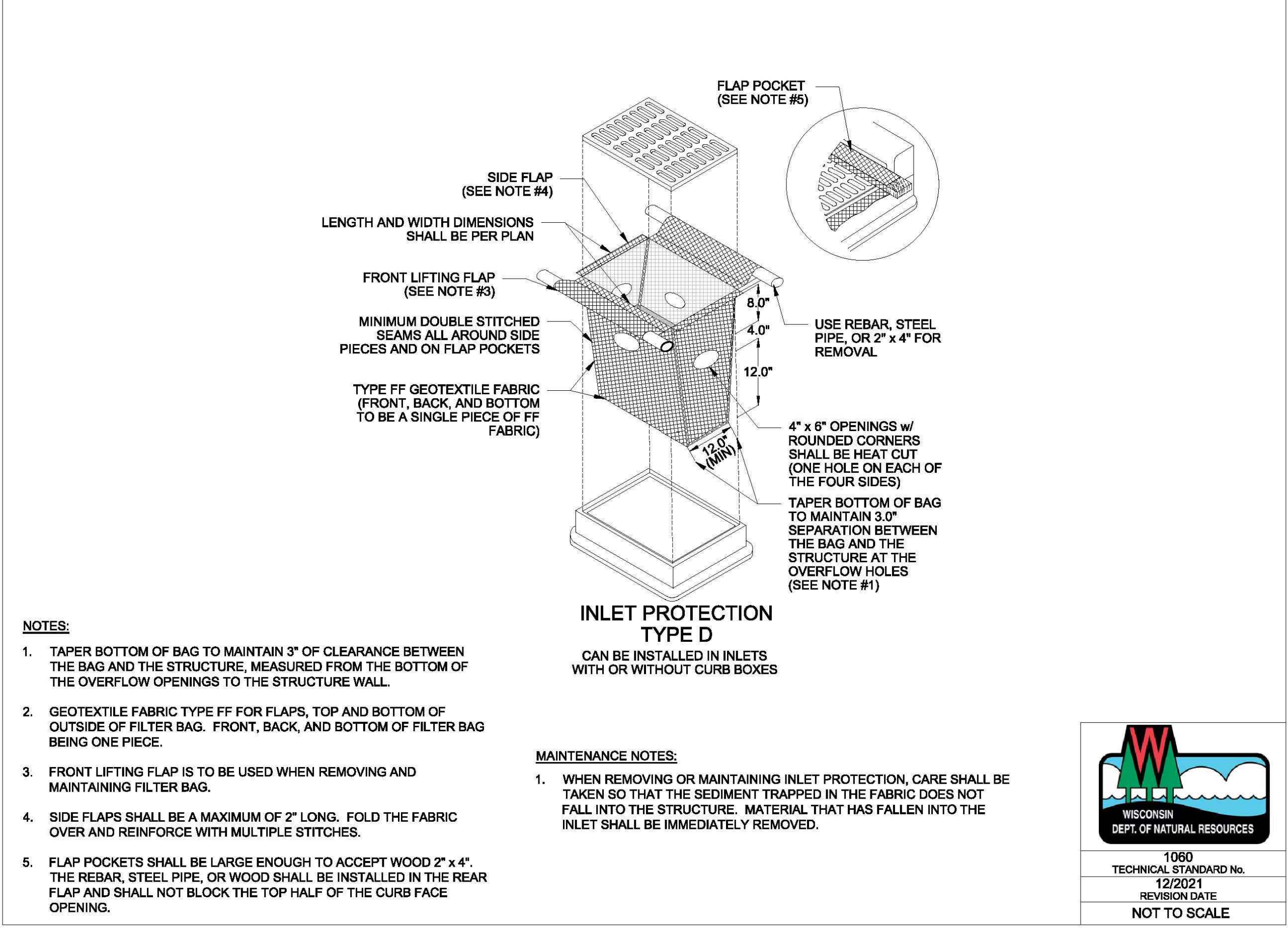


FIGURE 2. INLET PROTECTION TYPE D



DATE: 10/20/2025

REVISIONS: NO REVISIONS

NO. 1

Prepared For:

McDonald's
110 N. Carpenter St.
Chicago, IL 60607

McDONALD'S - FITCHBURG, WI
NWC of McKee Road and Fitchrona Road
Fitchburg, Wisconsin

Prepared By:

Watermark Engineering Resources
watermark-engineering.com | 2631 Ginger Woods Pkwy | Aurora, IL 60502 | (630) 375-1800

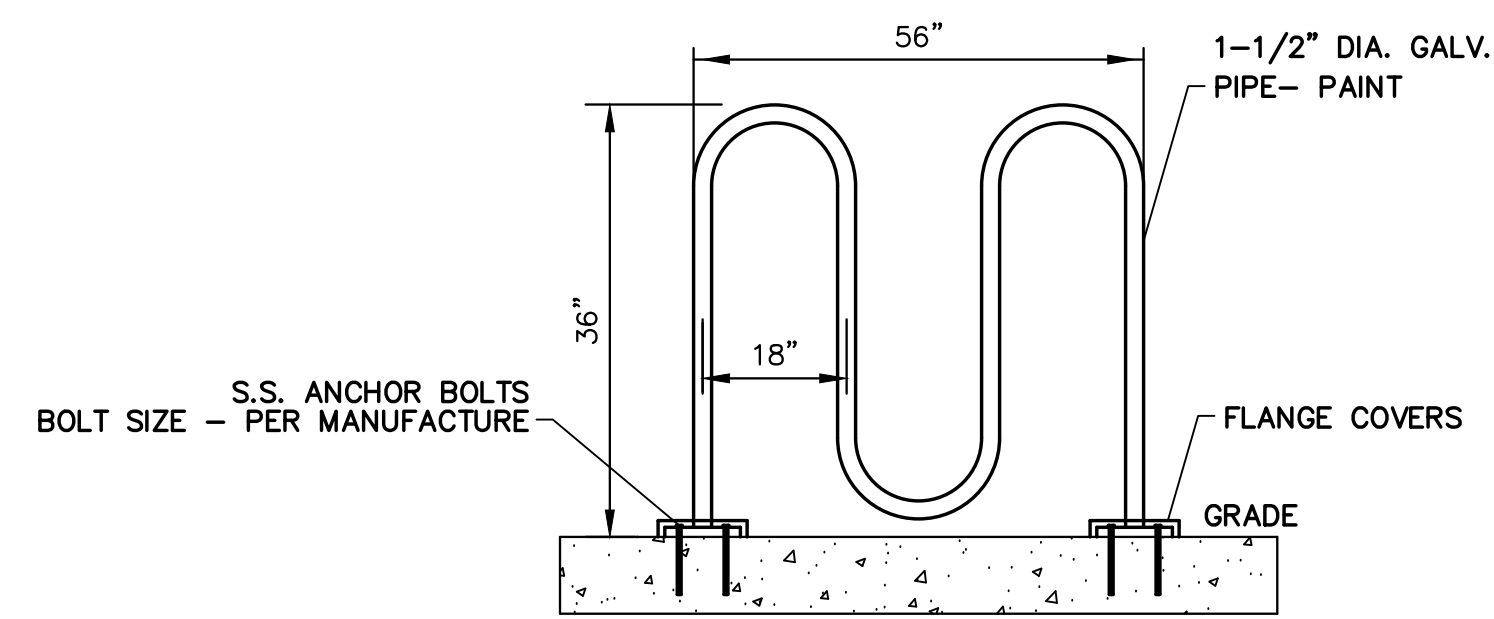
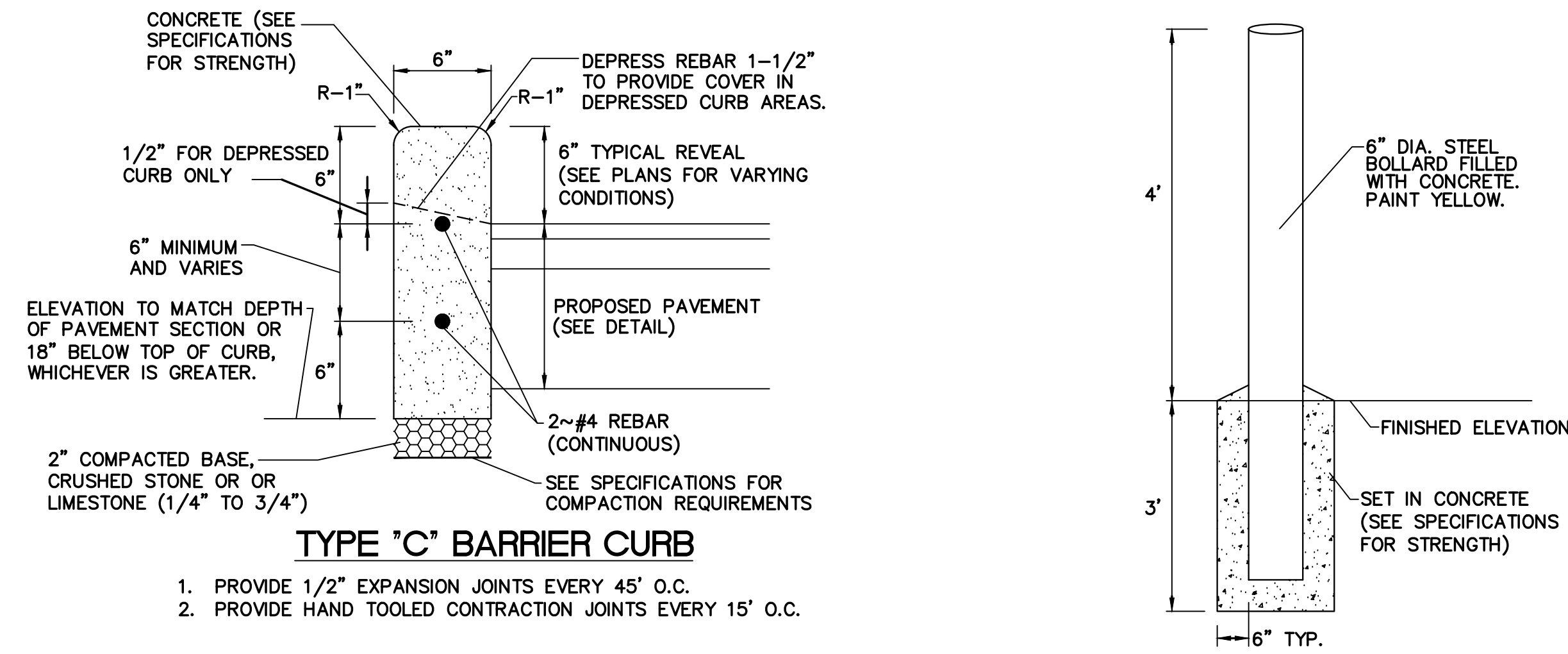
CHECKED BY: J. MILLER
DESIGN BY: J. VOLANTI
DRAWN BY: J. VOLANTI
DATE: DECEMBER 3, 2024
SCALE: NONE
PROJECT NO.: 24-001

1060
TECHNICAL STANDARD No.
12/2021
REVISION DATE
NOT TO SCALE

1060
TECHNICAL STANDARD No.
12/2021
REVISION DATE
NOT TO SCALE

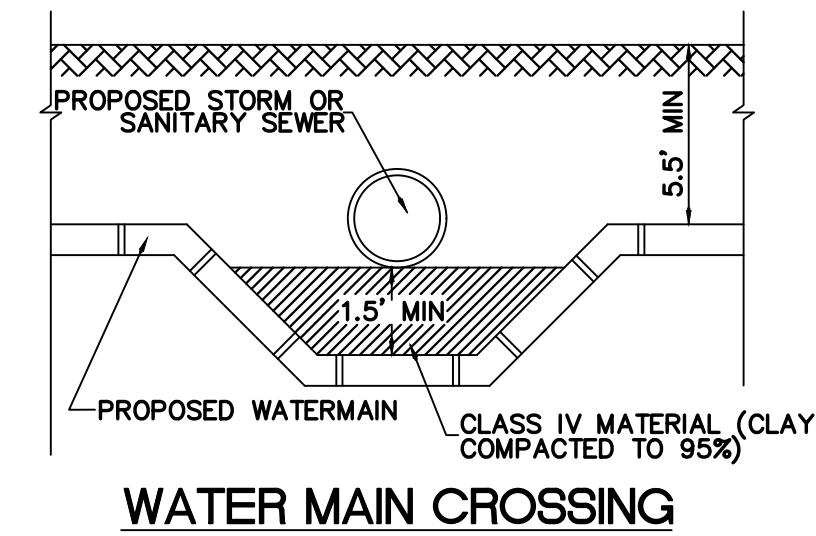
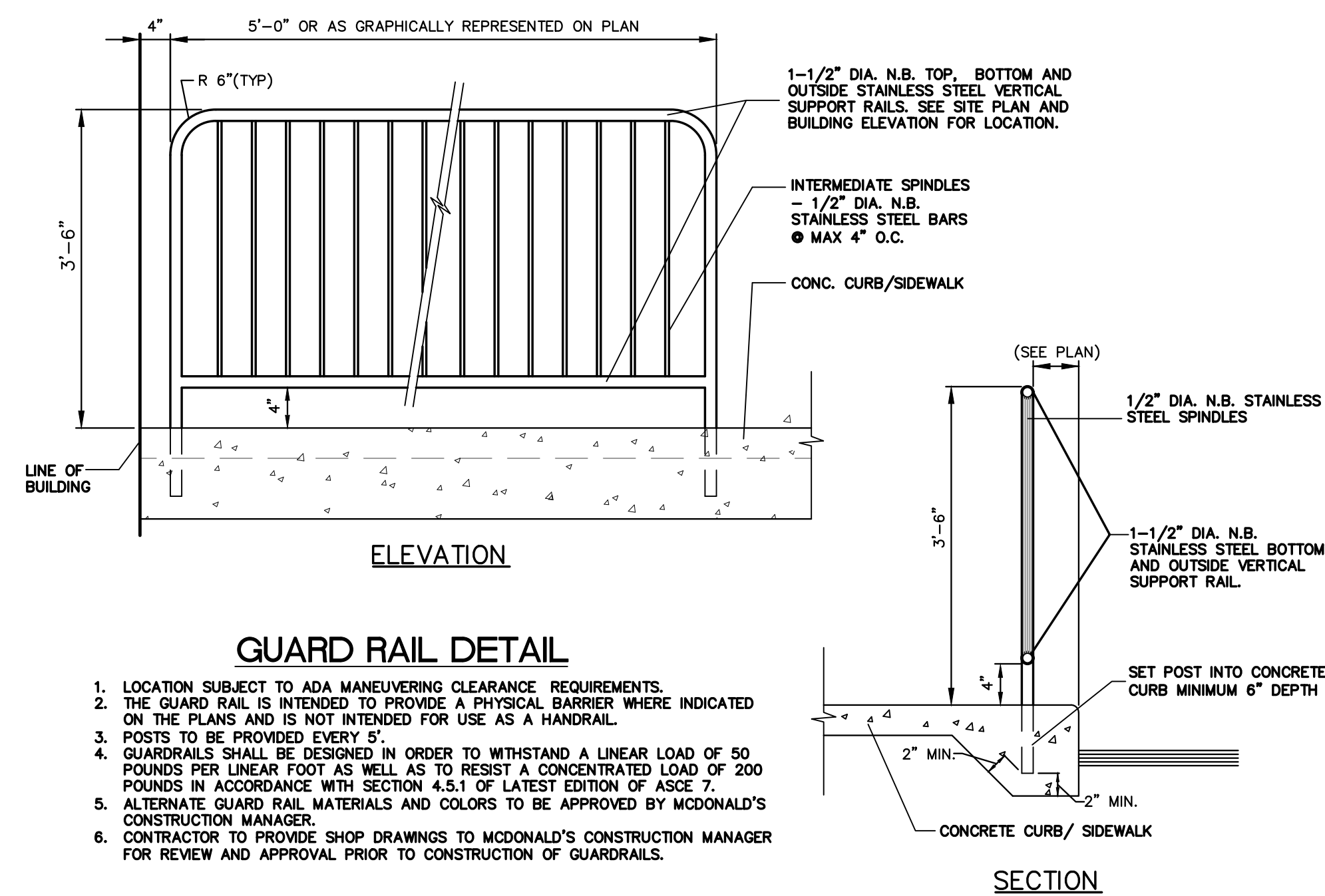
C-8
LC #48-1082

SOIL EROSION CONTROL DETAILS AND SPECS

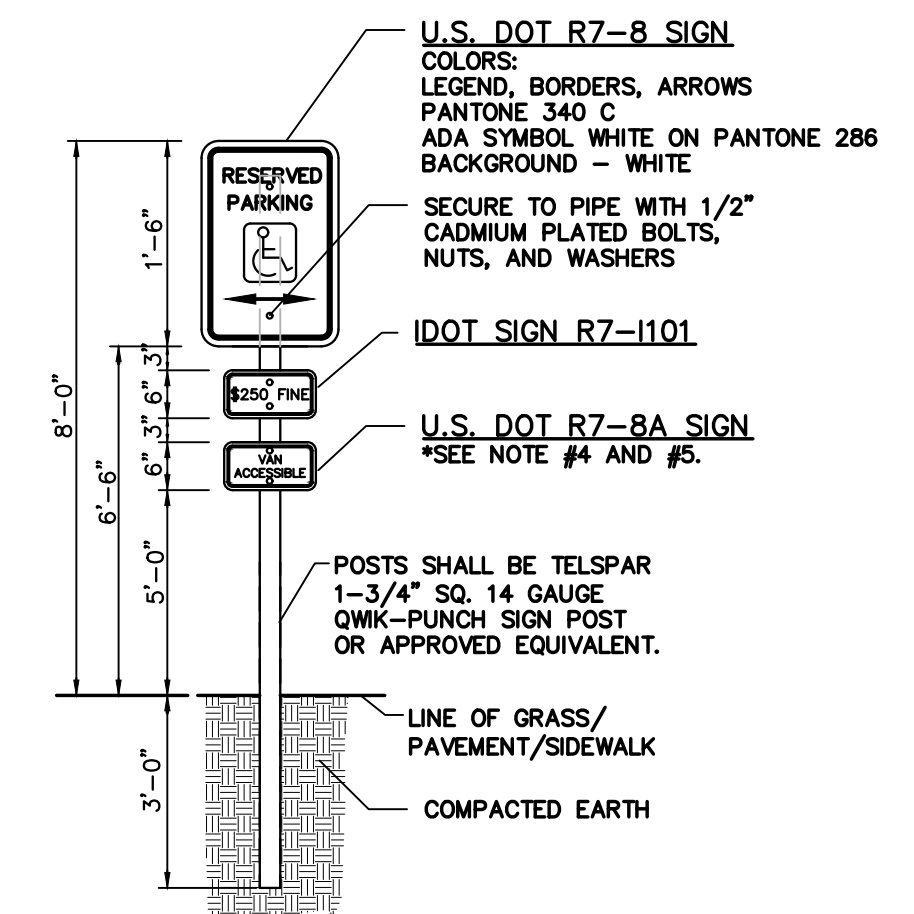
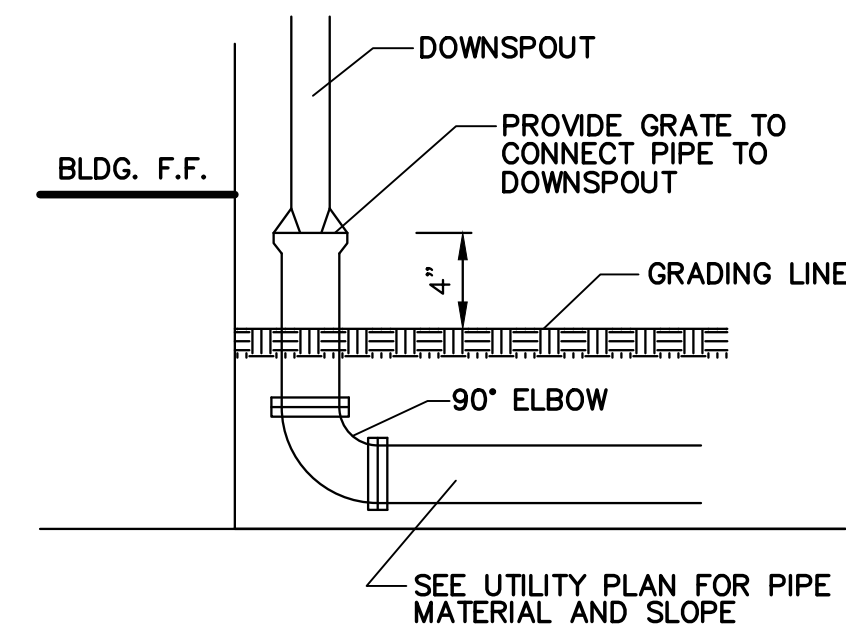


- * AS REQUIRED BY CODE OR TO ACHIEVE ADEQUATE SOIL BEARING- SEE DWG. S-1.
- SEE GEOMETRIC PLAN FOR LAYOUT AND PAVEMENT SECTIONS.
 - SEE GRADING PLAN FOR FINISH ELEVATIONS, ENSURE POSITIVE DRAINAGE.
 - CONTRACTOR TO VERIFY BOLT PATTERN AND INSTALL PER MANUFACTURER'S SPECIFICATIONS.

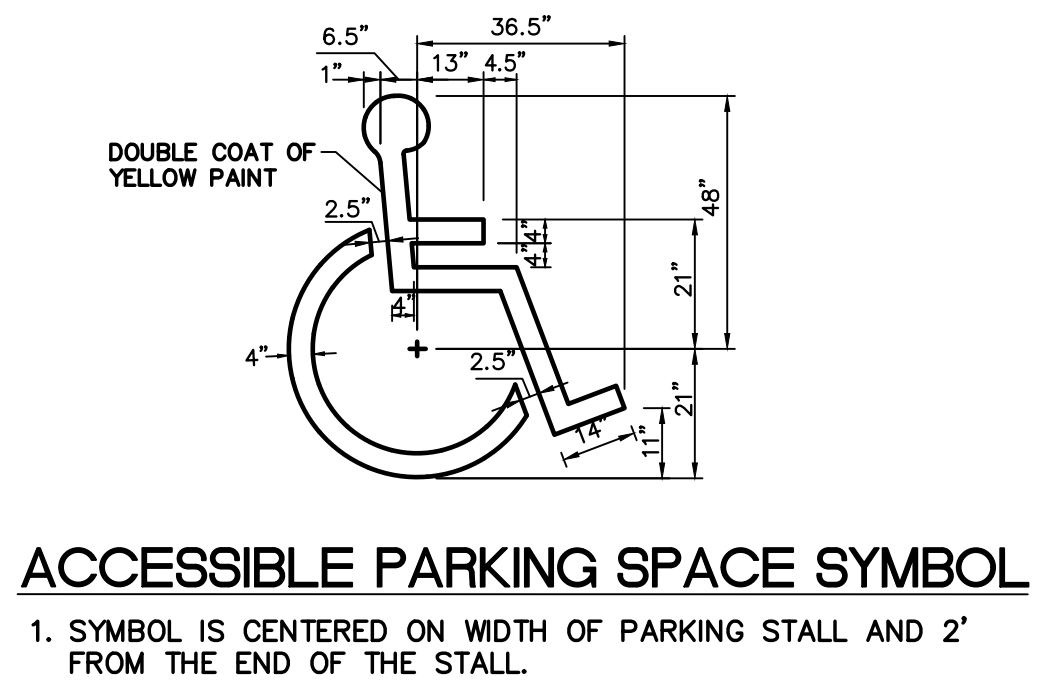
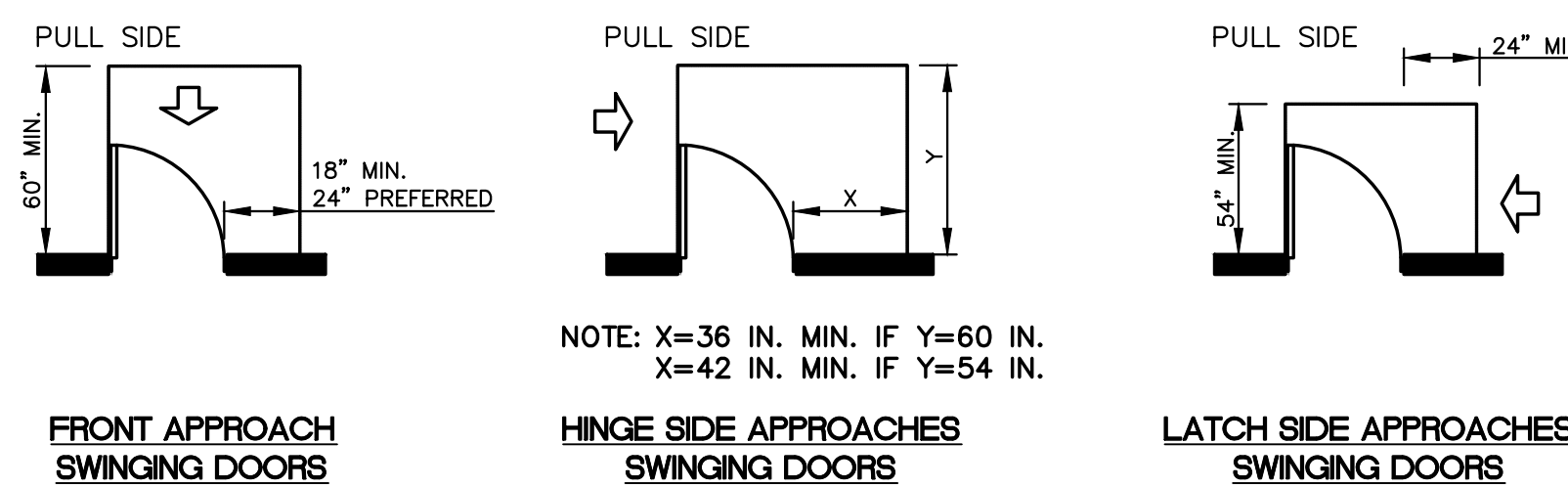
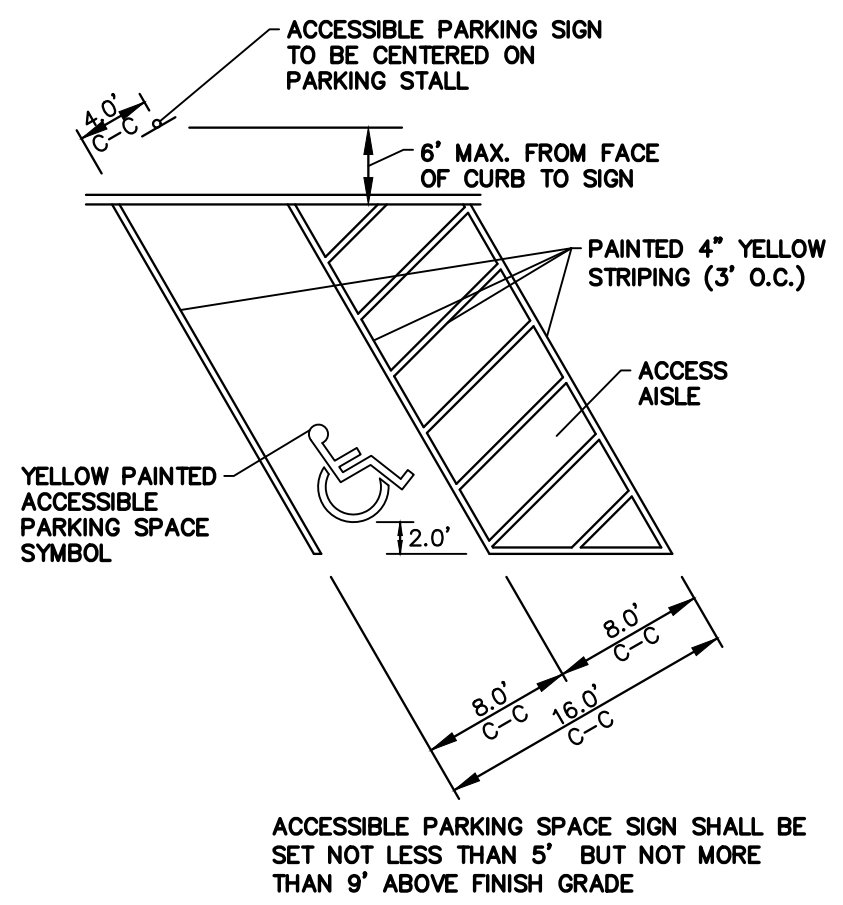
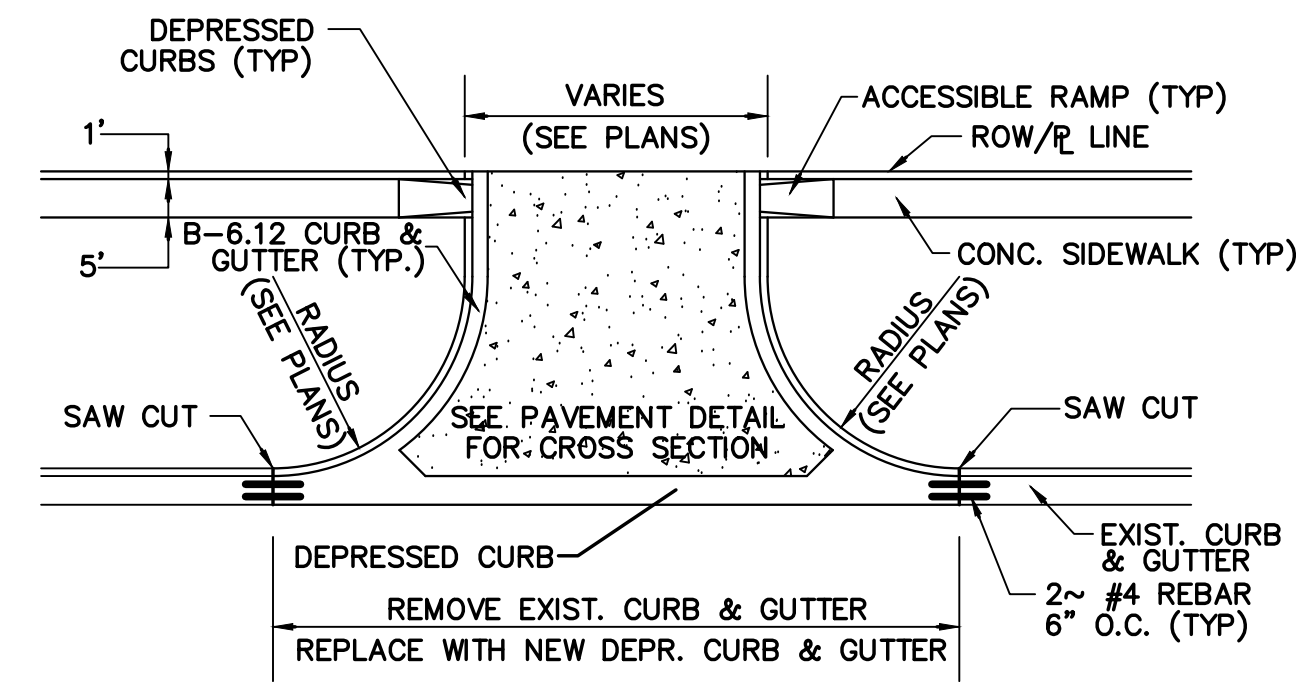
BICYCLE RIBBON RACK DETAIL



- NOTE:
- USE MIN. 21 L.F. OF WATERMAIN CLASS PIPES (MIN. 10' ON EACH SIDE OF THE CROSSING). SEE SPECIFICATIONS FOR PIPE AND JOINT SPECIFICATIONS.



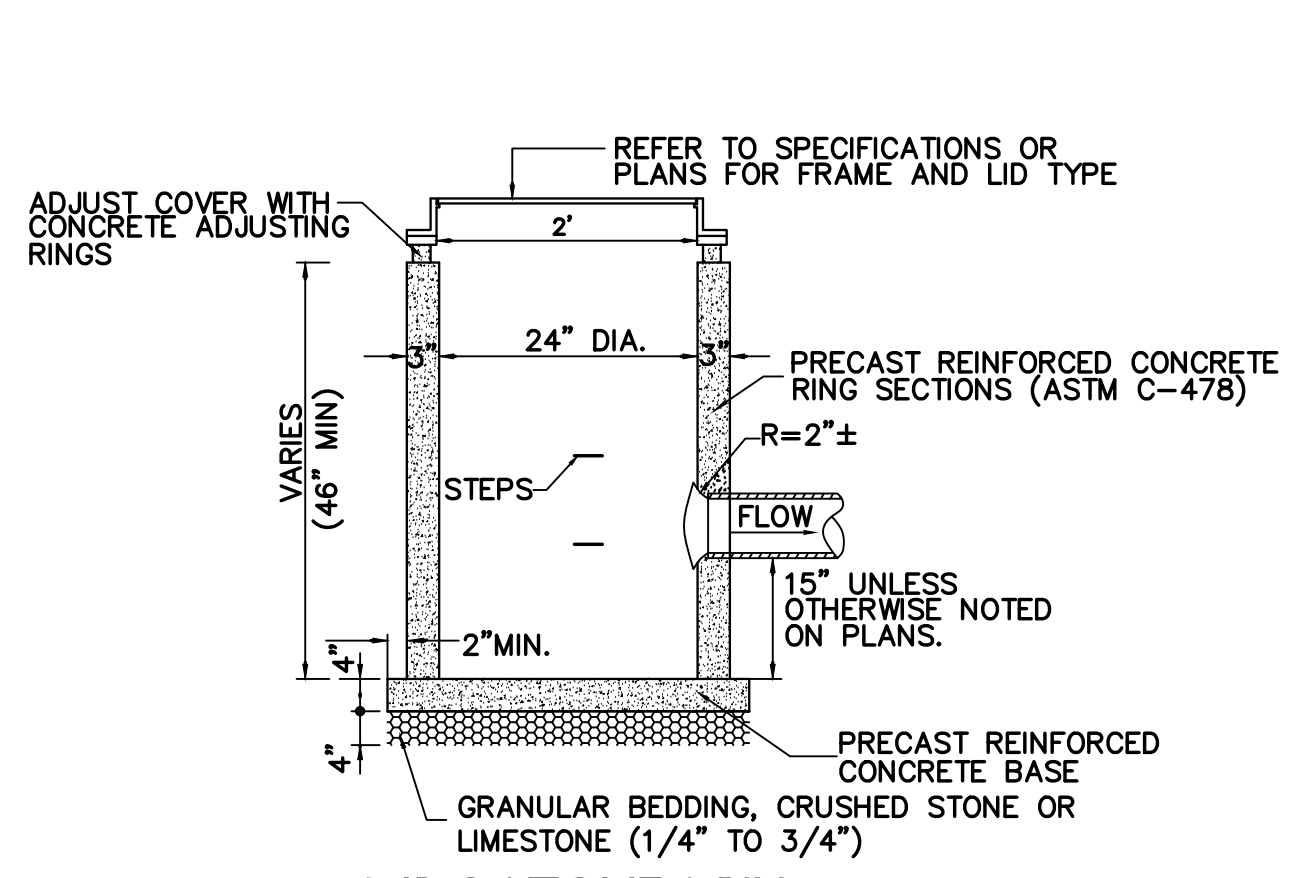
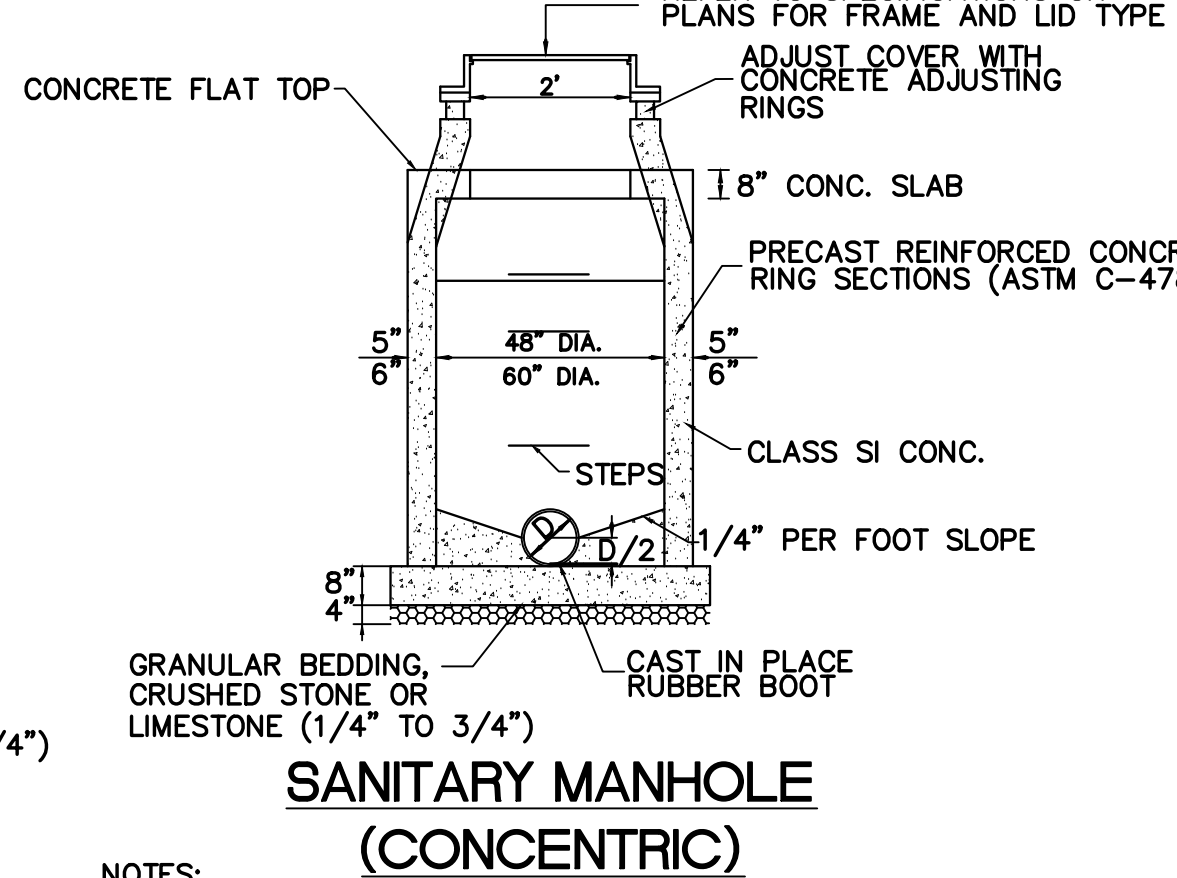
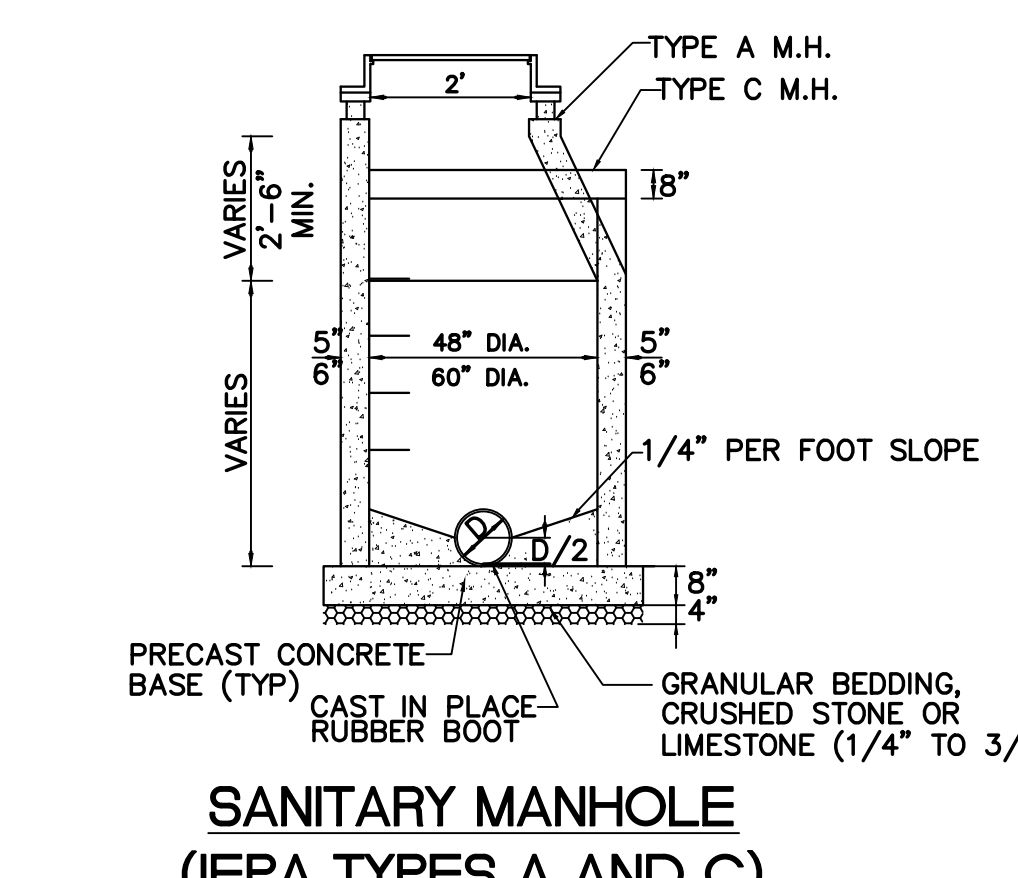
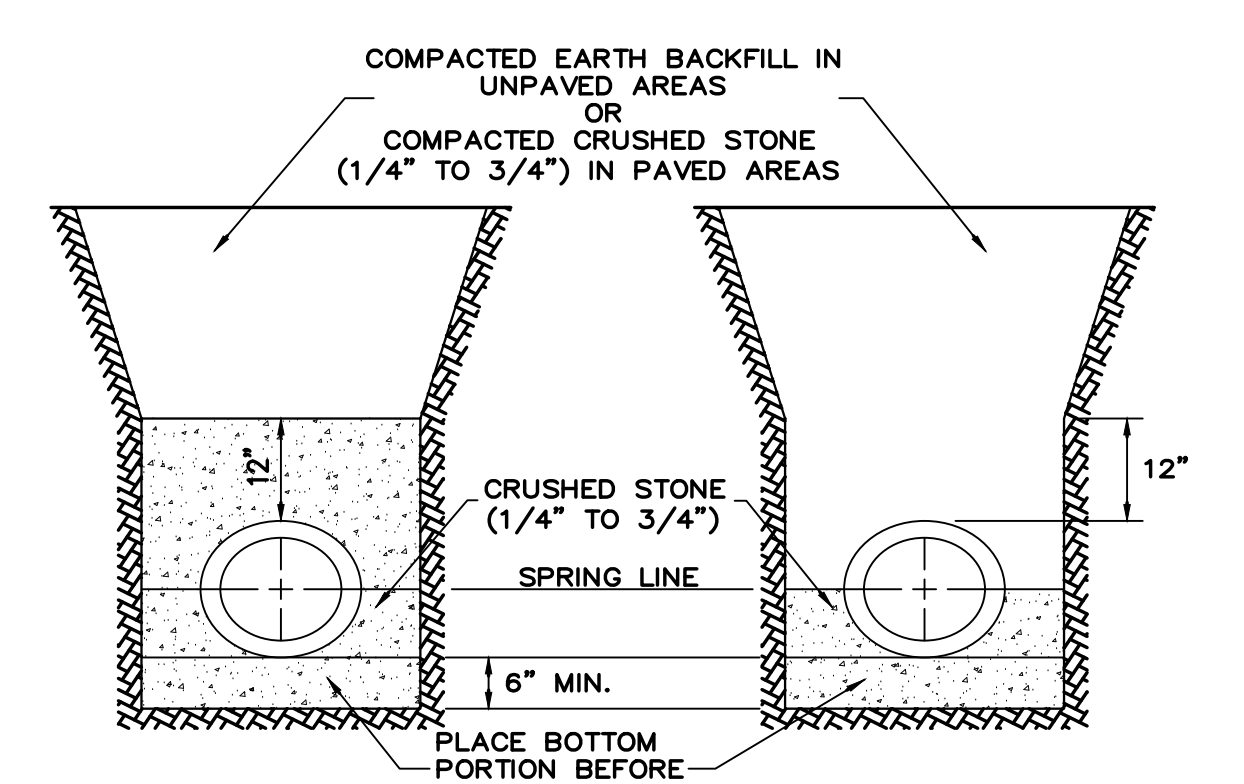
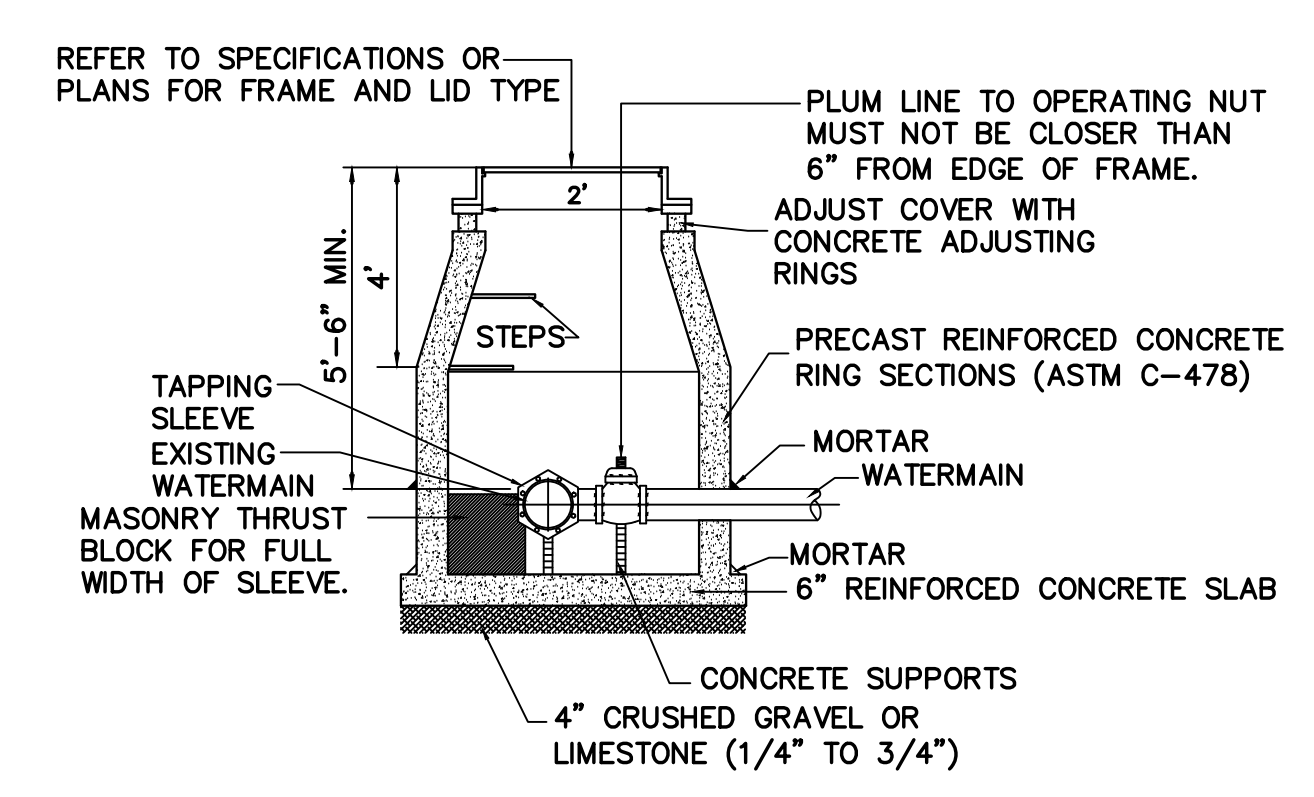
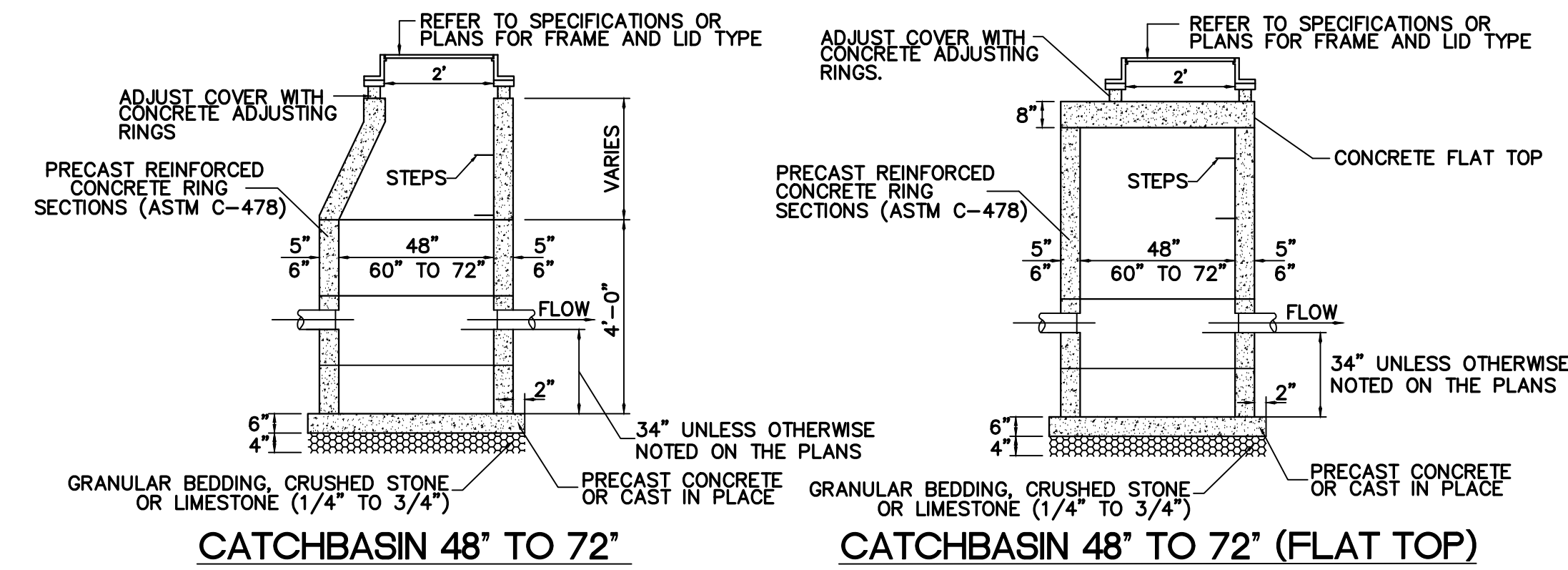
- WHERE A FINE IN EXCESS OF \$250 IS ESTABLISHED BY A MUNICIPALITY BY ORDINANCE IN ACCORDANCE WITH THE STATUTES, THE ACTUAL AMOUNT OF THE FINE SHOULD BE SHOWN. THIS PLATE MAY BE MOUNTED DIRECTLY BELOW THE R7-8 SIGN OR COMBINED WITH THAT SIGN ON A SINGLE 12 INCH BY 24 INCH PANEL.
 - ON THE RESERVED PARKING SIGN, THE ARROW SHOULD BE OMITTED WHERE THERE IS ONLY ONE SPACE. THE ARROW MAY ALSO BE MADE TO POINT IN ONLY ONE DIRECTION. THE ARROW MAY ALSO BE REPLACED BY "TIME" SUCH AS 9 AM-5 PM WHERE A PART TIME RESTRICTION EXISTS.
 - ONE IN EVERY SIX ACCESSIBLE SPACES, BUT NOT LESS THAN ONE, SHALL BE SERVED BY AN ACCESS AISLE 8 FEET WIDE MINIMUM AND SHALL BE DESIGNATED "VAN ACCESSIBLE".
 - THE LOWEST BOTTOM EDGE OF THE LOWEST REQUIRED SIGN SHALL BE MOUNTED AT 60" ABOVE FINISHED GRADE.
 - COLORS FOR BOTH IDOT SIGN R7-1101 AND USDOT R7-8A, LEGEND, BORDERS AND ARROWS TO BE GREEN, NON-REFLECTORIZED (PANTONE 340C) BACKGROUND-WHITE REFLECTORIZED.
- ACCESSIBLE PARKING SPACE SIGN DETAIL**



DATE	10/20/2025
REVISIONS	NO REVISIONS
NO.	1
Prepared For:	McDonald's 110 N. Carpenter St. Chicago, IL 60607
Project:	McDONALD'S - FITCHBURG, WI NWC of McKee Road and Fitchrona Road Fitchburg, Wisconsin
Prepared By:	Watermark Engineering Resources
Checked By:	J. MILLER
Design By:	J. VOLANTI
Drawn By:	J. VOLANTI
Date:	DECEMBER 5, 2024
Scale:	NONE
Project No.:	24-001
Watermark Engineering Resources	2631 Ginger Woods Pkwy Aurora, IL 60502 (630) 375-1800

PROJECT DETAILS

C-9
LC #48-1082



CLASS 1A
 (FLEXIBLE PIPE, ASTM 2321-89)
 (PVC, HDPE, CMP)

TYPE B
 (RIGID PIPE, ASTM C12)
 (RCP, DIP, CIP, VCP)

NOTES:
 1. GRANULAR BEDDING IS NOT REQUIRED FOR WATER MAIN; HOWEVER, SOIL BASE BEDDING MUST BE COMPACTED AND STABLE MATERIAL.
 2. FOUNDATION: WHERE THE TRENCH BOTTOM IS UNSTABLE, THE CONTRACTOR SHALL EXCAVATE TO A DEPTH REQUIRED BY THE ENGINEER AND REPLACE WITH A FOUNDATION OF CLASS I OR II MATERIAL AS DEFINED IN ASTM D2321, "STANDARD PRACTICE FOR INSTALLATION OF THERMOPLASTIC PIPE FOR SEWERS AND OTHER GRAVITY-FLOW APPLICATIONS," LATEST EDITION.
 3. MAXIMUM WIDTH OF TRENCH AT TOP OF PIPE FOR ALL CLASSES IS TO BE 4/3 INTERNAL DIAMETER PLUS 10" UNLESS SHEETING IS USED.
 4. MINIMUM COVER FOR PVC, HDPE, AND CMP IS 12" FROM THE TOP OF THE PIPE TO THE BOTTOM OF ASPHALT/OR TO TOP OF CONCRETE PAVEMENT, OR TO FINISHED GRADE ELEVATION IN LANDSCAPED AREAS.
 5. TRENCH BACKFILL MUST BE WELL SPADED ON BOTH SIDES OF PIPE.
 6. TRENCH BACKFILL AND BEDDING MATERIALS SHALL BE COMPACTED TO A MINIMUM OF 95% OF STANDARD LABORATORY DENSITY PER ASTM D698.

TRENCH DETAIL

NOTES:
 1. PROVIDE EXTERNAL CHIMNEY SEAL ON ALL SANITARY MANHOLES.
 2. USE TYPE A UNLESS SPECIFIED IN SEWER CALLOUT.
 3. OUTSIDE OF SANITARY MANHOLE SHALL BE MOISTURE PROOFED WITH TWO COATS OF BITUMINOUS MATERIAL.

NOTES:
 1. PROVIDE EXTERNAL CHIMNEY SEAL ON ALL SANITARY MANHOLES.
 2. USE TYPE A UNLESS SPECIFIED IN SEWER CALLOUT.
 3. OUTSIDE OF SANITARY MANHOLE SHALL BE MOISTURE PROOFED WITH TWO COATS OF BITUMINOUS MATERIAL.

NOTES:
 1. 60" MINIMUM DIAMETER FOR ALL PRESSURE CONNECTIONS.
 2. PIPE OPENINGS TO BE CAST INTO WALL.

FIXTURE INFORMATION:				
NAME	TYPE	FLOW RATE	OTHER	QTY.
3-COMP. SINK	3-COMPARTMENT SINK	GPM (1-MINUTE DRAIN TIME): 46.636 GPM (2-MINUTE DRAIN TIME): 21.818	DFU: 6 CAPACITY: 58.182 GAL.	1
FLOOR SINK/DRAIN	FLOOR SINK	GPM (1-MINUTE DRAIN TIME): 5 GPM (2-MINUTE DRAIN TIME): 2.5	DFU: 2 CAPACITY: 0 GAL.	11
MOP SINK - MS-1	MOP BASIN	GPM (1-MINUTE DRAIN TIME): 22.442 GPM (2-MINUTE DRAIN TIME): 11.221	DFU: 3 CAPACITY: 29.922 GAL.	2
VEGETABLE PREP SINK	PREP SINK ONE BOWL	GPM (1-MINUTE DRAIN TIME): 4.675 GPM (2-MINUTE DRAIN TIME): 2.338	DFU: 2 CAPACITY: 6.234 GAL.	1
WAREWASHER HOBART AM16SVLT-1	DOOR TYPE	GPM (1-MINUTE DRAIN TIME): 60 GPM (2-MINUTE DRAIN TIME): 30	DFU: 2 CAPACITY: 60 GAL.	1
TOTALS:		GPM (1-MINUTE DRAIN TIME): 208.19 GPM (2-MINUTE DRAIN TIME): 104.1	DFU: 38 CAPACITY: 184.26 GAL.	

SIZING DETAILS:

PROJECT INFORMATION
 CONSTRUCTIONS TYPE: NEW CONSTRUCTION
 ADDITIONAL NOTES: McDONALD'S 4584 BUILDING - PROTOTYPICAL

LOCATION: OUTDOORS
 BURIED: YES
 HIGH WATER AREA: NO

FLOW RATE CALCULATION:
 FLOW RATE TO INTERCEPTOR = (LESSER OF FLOW RATE BY PIPE SIZE AND FLOW RATE BY FIXTURES)
 SIZE OF PIPE CONNECTED TO THE GREASE INTERCEPTOR: 4 IN.
 DRAINAGE PERIOD: 2 MINUTES
 FLOW RATE BY PIPE SIZE: 75 GPM
 FLOW RATE BY FIXTURES, 2-MINUTE DRAINAGE PERIOD: 104.097 GPM
 FLOW RATE BY FIXTURES, 1-MINUTE DRAINAGE PERIOD: 208.195 GPM
 FLOW RATE USED TO SELECT GREASE INTERCEPTOR: 75 GPM
 FIXTURE DFU: 38

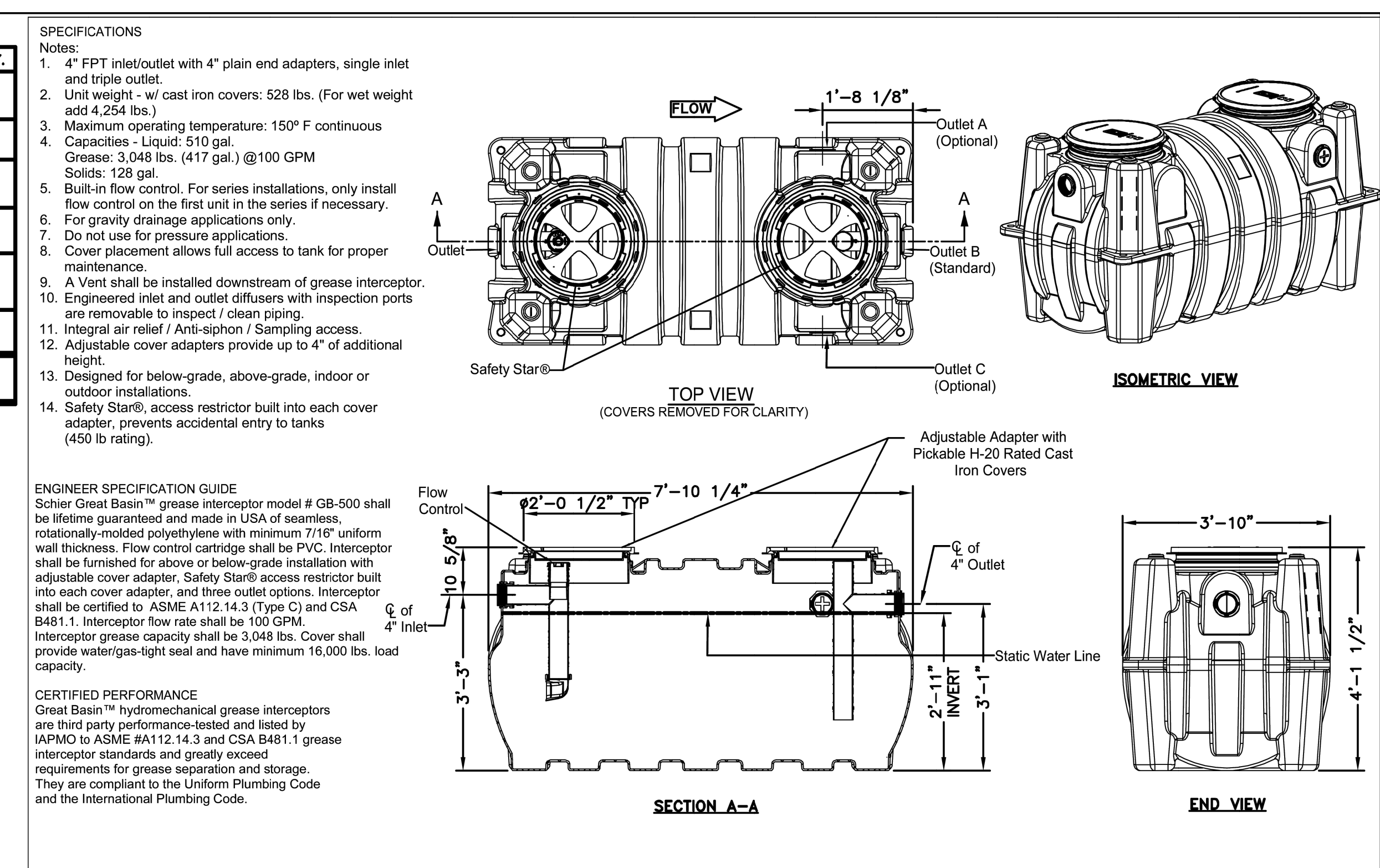
GREASE OUTPUT:
 MENU TYPE: FAST FOOD - FULL PREP
 FRYER IN KITCHEN: YES
 FOOD WASTE DISPOSER: YES
 NUMBER OF SEATS: 38 SEATS
 GREASE PRODUCTION VALUE: 0.035 LBS. PER SERVING (6.02 LBS. PER DAY)

30-DAY GREASE PRODUCTION: 180.6 LBS.
 60-DAY GREASE PRODUCTION: 361.2 LBS.
 90-DAY GREASE PRODUCTION: 541.8 LBS.

CALCULATION:
 GREASE OUTPUT=(NUMBER OF SEATS)x4(AVERAGE TURNS PER SEAT PER 24 HOURS)x(GREASE PRODUCTION VALUE)x(DAYS BETWEEN PUMP-OUT).

DETAIL

GREASE INTERCEPTOR
 SCALE: NONE



SPECIFICATION SHEET

MODEL NUMBER: GB-500
 PART NUMBER: 4075-001-01

DESCRIPTION: GB-500 GREASE INTERCEPTOR 100 GPM, 4" INLET/OUTLET, H-20 RATED CAST IRON COVERS

DWG BY: C. BUSENITZ DATE: 4/14/2022 REV: - ECO: -

SCHIER
 6455 Woodland Dr
 Shawnee, KS 66218
 Tel: 913-951-3300
 Fax: 913-851-3399
 schierproducts.com

PROJECT SPECIFICATIONS

- 1. CONTRACTOR IS TO FOLLOW ALL ORDINANCES AND REQUIREMENTS OF THE STATE, COMMUNITY, LOCAL DISTRICTS AND WISCONSIN DEPARTMENT OF TRANSPORTATION (WISDOT). ALL PROPOSED IMPROVEMENTS ARE TO BE CONSTRUCTED IN ACCORDANCE WITH THE "WISDOT", "STANDARD SPECIFICATIONS FOR ROAD, BRIDGE, AND MUNICIPAL CONSTRUCTION" CURRENT EDITIONS.
- 2. THE CONTRACTOR SHALL INDEMNIFY WATERMARK ENGINEERING RESOURCES, LTD (THE DESIGN ENGINEER), OWNER, THEIR AGENTS, ETC., FROM ALL LIABILITY INVOLVED WITH THE CONSTRUCTION. THE CONTRACTOR IS SOLELY RESPONSIBLE FOR CONDUCTING WORK IN ACCORDANCE WITH THE CONTRACT DOCUMENTS, SPECIFICATIONS, AND ALL GOVERNMENTAL AGENCIES HAVING JURISDICTION OVER THIS DEVELOPMENT.
- 3. THE CONTRACTOR IS RESPONSIBLE TO OBTAIN ALL PERMITS THAT ARE REQUIRED BY THE LOCAL AGENCIES.
- 4. PRIOR TO BID AND PRIOR TO CONSTRUCTION, THE CONTRACTOR SHALL INSPECT THE SITE TO VERIFY THAT THERE ARE NO DISCREPANCIES BETWEEN THE PLANS AND THE ACTUAL CONDITIONS AT THE SITE. IF ANY DISCREPANCIES ARE FOUND, AT ANY TIME BEFORE OR DURING CONSTRUCTION, THE CONTRACTOR SHALL NOTIFY THE DESIGN ENGINEER IMMEDIATELY (BEFORE ANY ADDITIONAL IMPROVEMENTS ARE INSTALLED) IN ORDER TO OBTAIN WRITTEN CONFIRMATION BY THE DESIGN ENGINEER AS TO ANY REVISIONS THAT MAY NEED TO BE MADE TO THE PLANS.
- 5. PRIOR TO CONSTRUCTION, CONTRACTOR IS TO CONTACT THE DESIGN ENGINEER TO VERIFY THAT THEY ARE WORKING FROM THE MOST CURRENT SET OF PLANS AND SPECIFICATIONS.
- 6. THE CONTRACTOR SHALL NOTIFY THE DESIGN ENGINEER, ALL GOVERNMENTAL AGENCIES HAVING JURISDICTION, AND ALL UTILITY COMPANIES THAT MAY BE AFFECTED BY THE PROPOSED CONSTRUCTION 48 HOURS PRIOR TO THE START OF CONSTRUCTION TO ARRANGE APPROPRIATE CONSTRUCTION INSPECTION.
- 7. THE MUNICIPALITY SHALL HAVE THE AUTHORITY TO INSPECT, APPROVE, AND REJECT THE CONSTRUCTION OF THE IMPROVEMENTS.
- 8. PRIOR TO CONSTRUCTION OF ANY IMPROVEMENTS, THE CONTRACTOR MUST CALL "DIGGERS HOTLINE" FOR THE LOCATION AND STAKING OF EXISTING UNDERGROUND UTILITIES (GAS, ELECTRIC, TELEPHONE) AT 811 (1-800-242-8511), OR SUBMIT A TICKET ONLINE AT "DIGGERSHOTLINE.COM" A MINIMUM OF 72 HOURS PRIOR TO DIGGING.
- 9. THE CONTRACTOR IS RESPONSIBLE FOR PROVIDING RECORD DRAWINGS PER THE MUNICIPALITY AND/OR ANY OTHER AGENCY REQUIREMENTS. ANY CHANGES TO THE DRAWINGS MUST BE REPORTED TO THE DESIGN ENGINEER BEFORE WORK PROGRESSES.
- 10. THE PROPOSED IMPROVEMENTS MUST BE CONSTRUCTED IN ACCORDANCE WITH THE ENGINEERING PLANS AS APPROVED BY THE MUNICIPALITY.
- 11. ALL QUANTITIES ARE THE RESPONSIBILITY OF THE CONTRACTOR AND ARE TO BE VERIFIED PRIOR TO CONSTRUCTION. IF DISCREPANCIES OCCUR, THE CONTRACTOR IS TO CONTACT THE DESIGN ENGINEER IMMEDIATELY AND NO WORK IS TO BE DONE UNTIL APPROVED BY THE DESIGN ENGINEER.
- 12. ANY RESTORATION NEEDED BECAUSE OF CONSTRUCTION SHALL BE PROVIDED BY THE CONTRACTOR AT NO ADDITIONAL COST.
- 13. TRENCH BACKFILL MATERIAL (3/4" MAX CRUSHED AGGREGATE WITH FINES) IS REQUIRED UNDER AND WITHIN TWO FEET (2') OF SIDEWALKS AND PROPOSED PAVED AREAS. THIS BACKFILL SHALL BE IN SIX INCH (6") LIFTS AND COMPACTED TO 95% STANDARD PROCTOR.
- 14. CONTRACTOR IS TO PROVIDE ALL TEMPORARY SIGNAGE AS REQUIRED BY THE WISCONSIN DEPARTMENT OF TRANSPORTATION AND LOCAL MUNICIPALITIES.
- 15. ALL EXISTING DRAIN TILES THAT ARE ENCOUNTERED ARE TO BE RESTORED TO THEIR ORIGINAL CONDITION OR REROUTED TO THE PROPOSED STORM SEWER SYSTEM.
- 16. RESTORATION OF EXISTING RIGHT-OF-WAYS IS TO BE COMPLETED WITH FOUR INCH (4") MINIMUM TOPSOIL AND SALT TOLERANT SOD UNLESS OTHERWISE NOTED.
- 17. THE WATER SYSTEM CANNOT BE SHUT DOWN WITHOUT CONSENT BY THE MUNICIPALITY.
- 18. ALL FRAME ADJUSTMENTS SHALL BE MADE WITH PRE-CAST CONCRETE RINGS CONFORMING TO ASTM C-39 AND CANNOT EXCEED TWELVE INCHES (12").
- 19. FRAMES SHALL BE SET WITH EZ STIK8 (OR EQUAL) MATERIAL TO PREVENT LEAKAGE.
- 20. THE REINFORCED CONCRETE SECTIONS SHALL BE LAID IN MORTAR, SEALED WITH EXTERNAL SEALING BANDS, OR SEALED USING MASTIC JOINT SEALER. WHEN MASTIC JOINT SEALER IS USED, THE MATERIAL SHALL COMPLETELY FILL THE JOINT AFTER THE UNITS HAVE BEEN BROUGHT TOGETHER.
- 21. STEPS IN STRUCTURES SHALL BE MADE OF COPOLYMER POLYPROPYLENE PLASTIC WITH ONE HALF INCH (1/2") GRADE SIXTY (60") STEEL REINFORCEMENT, STEP PSI-PF, AS MANUFACTURED BY M.A. INDUSTRIES, INC., OR APPROVED EQUAL. STEPS TO BE SPACED SIXTEEN INCHES (16") ON-CENTER.
- 22. ALL INSTRUMENTS ARE TO BE PROPERLY CALIBRATED PRIOR TO CONSTRUCTION USE.
- 23. ALL PARKING LOT LIGHT POLES ARE TO BE CONSTRUCTED AT THE INTERSECTION OF PARKING LOT STRIPING OR IN LANDSCAPE AREAS WITH A MINIMUM OF 2' CLEARANCE BETWEEN THE BACK OF CURB AND THE EDGE OF THE PARKING LOT LIGHT BASE UNLESS OTHERWISE SPECIFIED.
- 24. GENERAL CONTRACTOR TO BE BECOME FAMILIAR WITH AND APPLY THE ADA MINIMAL REQUIREMENTS AND REPORT TO THE DESIGN ENGINEER ANY DISCREPANCIES BEFORE CONSTRUCTION. THIS INCLUDES, BUT NOT LIMITED TO, TRANSITIONS TO EXISTING CONDITIONS.
- 25. CONSTRUCTION MEANS, METHODS AND JOB SITE SAFETY IS THE SOLE AND EXCLUSIVE RESPONSIBILITY OF THE CONTRACTOR.
- 26. PAVING, SIDEWALK, AND CURBING IS NOT TO BE INSTALLED IN SUCH A WAY THAT IT WILL BLOCK THE FLOW OF WATER AWAY FROM THE BUILDING INCLUDING BUT NOT LIMITED TO WEEP HOLES, WICKS, DRAINAGE SCUPPERS OR PIPES, AND LANDSCAPE.

PAVEMENT

- 1. ALL PAVEMENTS SHALL BE DESIGNED AND CONSTRUCTED IN ACCORDANCE WITH THE FOLLOWING REFERENCES AS THEY APPLY: WISCONSIN DEPARTMENT OF TRANSPORTATION (WISDOT), STANDARD SPECIFICATIONS LATEST EDITION.
- 2. ALL PAVED AREAS SHALL BE COMPACTED TO 95% STANDARD LABORATORY DENSITY. PER WISCONSIN DEPARTMENT OF TRANSPORTATION. BEFORE THE BASE COURSE MATERIALS ARE INSTALLED, THE SUB-BASE SHALL BE PROOF-ROLLED TO THE SATISFACTION OF THE ENGINEER, HIS AGENT, AND/OR THE SOILS ENGINEER. COMPACTION AND DENSITY TESTS SHALL BE TAKEN AT THE OWNER'S OPTION.
- 3. ALL CONCRETE TO BE MINIMUM 3500 PSI, 6 BAG MIX WITH A SPRAY ON SEALER. EXPANSION AND CONTRACTION JOINTS SHALL BE TOOL FINISHED.
- 4. BINDER COURSE TO BE PLACED WHEN TEMPERATURE IS AT LEAST 40° F AND RISING. SURFACE COURSE TO BE PLACED WHEN TEMPERATURE IS AT LEAST 45° AND RISING.
- 5. ALL PROPOSED PAVEMENT AND CURBS ARE TO BE CONSTRUCTED TO WITHIN A TOLERANCE OF 0.05' OF THE PROPOSED ELEVATIONS EXCEPT IN THE ACCESSIBLE STALLS OR ACCESSIBLE ROUTES.
- 6. PRIOR TO SEAL COATING, ALL ASPHALT AREAS ARE TO BE CLEAN AND DRY. ALL LOOSE MATERIALS ARE TO BE REMOVED. ALL GREASE TO BE REMOVED. ALL CRACKS ARE TO BE FILLED PER WISDOT, STANDARD SPECIFICATION, SECTION 475. ALL PAINTED STRIPING TO BE MODIFIED SHALL BE "BLACKED OUT" WITH BLACK PAINT (1 COAT MINIMUM, 2 COATS IF NECESSARY), ALLOWED TO THOROUGHLY DRY PER PAINT MANUFACTURER, PRIOR TO SEAL COATING. ALL AREAS THAT ARE ADJACENT TO THE SEAL COATED AREA ARE TO BE MASKED (I.E. SIDEWALKS, CONCRETE SURFACES, BRICK SURFACES, GUTTERS, CATCHBASINS/INLETS, ETC.) PRIOR TO SEAL COATING TO BE APPLIED. AIR TEMPERATURE TO BE 50°F AND RISING. APPLICATION RATE TO BE SUCH THAT ALL SURFACES OF THE ASPHALT BEING COATED IS THOROUGHLY COVERED IN ONE COAT. SPRAYING IS NOT ALLOWED. ALL SEAL COATING SHOULD BE APPLIED BY SQUEEGEE OR BRUSHES. THE BITUMINOUS SEAL COATING MATERIAL SHOULD NOT BE ALLOWED TO ENTER STORM SEWERS AND SHOULD BE ALLOWED TO DRY AT LEAST 18 HOURS PRIOR TO VEHICULAR USE. CRACK FILLER AND SEAL COATING MATERIALS ARE TO BE FREE OF COAL TAR.

WATER MAIN SPECIFICATIONS

- 1. HORIZONTAL SEPARATION
A. WATER MAINS AND SEWERS: WATER MAINS SHALL BE LAID AT LEAST TEN FEET HORIZONTALLY FROM ANY EXISTING OR PROPOSED DRAIN, STORM SEWER, SANITARY SEWER, COMBINED SEWER OR SEWER SERVICE CONNECTION.
B. WATER MAINS MAY BE LAID CLOSER THAN TEN FEET TO A SEWER LINE OR SEWER SERVICE CONNECTION WHEN:
i) LOCAL CONDITIONS PREVENT A LATERAL SEPARATION OF TEN FEET;
ii) THE WATER MAIN INVERT IS AT LEAST EIGHTEEN INCHES (18") ABOVE THE CROWN OF THE SEWER; AND
iii) THE WATER MAIN IS EITHER IN A SEPARATE TRENCH OR IN THE SAME TRENCH ON AN UNDISTURBED EARTH SHELF LOCATED TO ONE SIDE OF THE SEWER.
C. BOTH THE WATER MAIN AND SEWER PIPE SHALL BE CONSTRUCTED OF PUSH JOINT OR MECHANICAL JOINT DUCTILE IRON PIPE, PRESSURE PIPE, PRESTRESSED CONCRETE PIPE, OR PVC SDR18 PIPE WITH AWWA C-900 JOINTS, WHEN IT IS IMPOSSIBLE TO MEET (A) OR (B) ABOVE, THE DRAIN OR SEWER SHALL BE PRESSURE TESTED TO THE MAXIMUM EXPECTED SURCHARGE HEAD BEFORE BACKFILLING.
- 2. VERTICAL SEPARATION
A. A WATER MAIN SHALL BE LAID SO THAT ITS INVERT IS EIGHTEEN INCHES (18") ABOVE THE CROWN OF THE DRAIN OR SEWER WHENEVER WATER MAINS CROSS STORM SEWERS, SANITARY SEWERS OF SEWER SERVICE CONNECTIONS. THE VERTICAL SEPARATION SHALL BE MAINTAINED FOR THAT PORTION OF THE WATER MAIN LOCATED WITHIN TEN FEET HORIZONTALLY OF ANY SEWER OR DRAIN CROSSED. A LENGTH OF WATER MAIN PIPE SHALL BE CENTERED OVER THE SEWER TO BE CROSSED WITH JOINTS EQUIDISTANT FROM THE SEWER OR DRAIN.
B. BOTH THE WATER MAIN AND SEWER SHALL BE CONSTRUCTED OF PUSH JOINT OR MECHANICAL JOINT DUCTILE PIPE, PRESTRESSED CONCRETE PIPE, OR PVC PIPE AND CONSTRUCTION SHALL EXTEND ON EACH SIDE OF THE CROSSING UNTIL THE PERPENDICULAR DISTANCE FROM THE WATER MAIN TO THE SEWER DRAIN LINE IS AT LEAST TEN FEET (10') WHEN:
i) IT IS IMPOSSIBLE TO OBTAIN THE PROPER VERTICAL SEPARATION AS DESCRIBED IN (A) ABOVE; OR
ii) THE WATER MAIN PASSES UNDER A SEWER OR DRAIN.
C. A VERTICAL SEPARATION OF EIGHTEEN INCHES (18") BETWEEN THE INVERT OF THE SEWER OR DRAIN AND THE CROWN OF THE WATER MAIN SHALL BE MAINTAINED WHERE A WATER MAIN CROSSES UNDER A SEWER. SUPPORT THE SEWER OR DRAIN LINES TO PREVENT SETTling AND BREAKING THE WATER MAIN.
- 3. WATER MAINS AND SERVICES SHALL BE CONSTRUCTED SO THAT THE MINIMUM DEPTH IS FIVE AND ONE HALF FEET (5 1/2') MEASURED FROM FINISHED GRADE TO THE TOP OF THE PIPE, UNLESS OTHERWISE SPECIFIED AND/OR APPROVED BY THE REVIEW ENGINEER.
- 4. ALL WATER MAIN FITTINGS MAY OR MAY NOT BE SHOWN ON THE PLANS AND SHOULD BE INCLUDED IN THE COST OF THE WATER MAIN ITSELF FOR BIDDING PURPOSES. ALL WATER MAIN SHALL BE DUCTILE IRON CLASS 52 CEMENT LINED CONFORMING TO ANSI A-21.51 WITH ANSI A-21.11 JOINTS, OR TYPE "K" COPPER PIPE WITH SWEATED JOINTS.
- 5. FIRE HYDRANTS SHALL MEET AWWA C-502 AND BE TRAVERSE CITY IRON WORKS, EAST JORDAN 5 BR, OR APPROVED EQUAL, WITH FIVE AND ONE QUARTER INCH (5 1/4") VALVE OPENING, TWO TWO AND ONE HALF INCH (2 1/2") HOSE NOZZLES AND ONE FIVE INCH (5") PUMPER NOZZLE. FIRE HYDRANT SHALL BE EQUIPPED WITH AN AUXILIARY RESILIENT SEAL GATE VALVE COMPLETE WITH ROADWAY BOX, TYLER, 6850 SERIES, ITEM 668-S. FIRE HYDRANTS MUST HAVE THEIR DISCHARGE AT LEAST 18 INCHES (18") BUT NOT MORE THAN TWENTY-FOUR INCHES (24") FROM THE SURFACE OF THE ADJACENT GROUND.
- 6. HYDRANTS SHALL BE INSTALLED NO CLOSER THAN THREE FEET (3') NOR FURTHER THAN EIGHT FEET (8') FROM THE BACK OF CURB OR EDGE OF PAVEMENT TO THE FIVE INCH (5") STEAMER NUT. NO BARRIERS, TREES, BUSHES, WALLS OR OTHER OBSTACLES WHICH MAY HIDE OR IMPEDE THE USE OF A FIRE HYDRANT SHALL BE INSTALLED, MAINTAINED, CONSTRUCTED, OR ENLARGED, WITHIN FORTY-EIGHT INCHES (48") OF A HYDRANT.
- 7. ALL STRUCTURE LIDS SHALL BE IMPRINTED "WATER".
- 8. ALL WATERTIGHT FRAMES AND LIDS SHALL BE NEENAH R-1916-C ALL OTHER FRAMES AND LIDS SHALL BE NEENAH R-1550-A WITH A CONCEALED PICK HOLE.
- 9. ALL FRAMES AND LIDS SHALL CONFORM TO ASTM A-48.
- 10. BEFORE BEING PLACED INTO SERVICE, ALL NEW MAINS AND REPAIRED PORTIONS OF, OR EXTENSIONS TO EXISTING MAINS SHALL BE CHLORINATED SO THAT THE INITIAL CHLORINE RESIDUAL IS NOT LESS THAN FIFTY (50) mg/L AND THAT A CHLORINE RESIDUAL OF NOT LESS THAN TWENTY-FIVE (25) mg/L REMAINS IN THE WATER AFTER STANDING TWENTY-FOUR (24) HOURS IN THE PIPE.

GRADING

- 1. GEOTECHNICAL REPORTS AS PREPARED BY OWNER (OR REPRESENTATIVE) SHALL BE REFERRED TO PRIOR TO EARTH MOVING AND/OR UTILITY CONSTRUCTION.
- 2. UNSTABLE SOIL SHALL BE REMOVED OR STABILIZED.
- 3. CONTRACTOR IS TO MAINTAIN A POSITIVE DRAINAGE PATTERN AT THE END OF EACH DAY. CARE SHOULD BE TAKEN TO INSURE THAT DRAINAGE IS NOT REROUTED OR BLOCKED IN A WAY THAT MAY BE INJURIOUS TO ADJACENT LAND.
- 4. SUB-BASES BELOW STRUCTURES, PAVEMENTS OR NEW STRUCTURAL FILL SHALL BE PROOF ROLLED. IF SOIL RUTS, PUMPS, DEFLECTS EXCESSIVELY OR EXHIBITS EXCESSIVE MOVEMENT OR MOISTURE, THEN THE UNSTABLE SOIL SHALL BE UNDERCUT AND REPLACED WITH STRUCTURAL FILL OR OTHERWISE STABILIZED IN A MANNER THAT MEETS THE COMPACTION REQUIREMENTS. THIS PROCESS IS TO BE OBSERVED BY A GEOTECHNICAL ENGINEER.
- 5. ALL FILLS SHALL BE PLACED IN 8" LIFTS COMPACTED TO A MINIMUM OF 98% STANDARD LABORATORY DENSITY PER ASTM D698 UNDER AND WITHIN INFLUENCE OF THE BUILDING, A MINIMUM OF 95% STANDARD LABORATORY DENSITY PER ASTM D698 UNDER AND WITHIN THE INFLUENCE OF ALL OTHER IMPERVIOUS AREAS, AND A MINIMUM OF 90% STANDARD LABORATORY DENSITY PER ASTM D698 IN ALL LANDSCAPE AREAS.
- 6. EROSION CONTROL SHALL BE PROVIDED PRIOR TO ANY DISTURBANCES.
- 7. PROVIDE TOPSOIL RESPREAD PER THE FOLLOWING UNLESS OTHERWISE NOTED:
A. 4" MINIMUM IN GRASS OR SOD AREAS.
B. 6" MINIMUM IN PLANTING AREAS.
C. 12" MINIMUM IN LANDSCAPE ISLANDS.
- 8. ALL TOPSOIL TO BE FRIABLE (NOT COHESIVE), WEED FREE, AND FREE OR ROCKS, LARGE ROOTS AND UNNATURAL DEBRIS.
- 9. ALL GRADING IS TO BE CONSTRUCTED TO WITHIN A TOLERANCE OF 0.10' OF THE PROPOSED ELEVATION OR SUB-BASE ELEVATION.
- 10. PRIOR TO UTILITY CONSTRUCTION, PROPOSED PAVEMENT AREAS, BUILDING PADS, DRIVEWAYS, SIDEWALKS AND YARD/OPEN SPACE AREAS SHALL BE ROUGH EXCAVATED OR FILLED TO PLUS OR MINUS ONE FOOT (1.0') OF DESIGN SUB-BASE ELEVATIONS.
- 11. THE FILL AREAS WITHIN THE RIGHT-OF-WAY WILL REQUIRE SOIL COMPACTION TESTING BY A GEOTECHNICAL FIRM PER W.S.D.O.T. SPECIFICATIONS.

SANITARY SEWER SPECIFICATIONS:

- 1. ALL SANITARY SEWER PIPE SHALL BE D.I.P MIN. CLASS 50, CONFORMING TO ANSI A-21.51 WITH ANSI 21.11 JOINTS; OR P.V.C. PIPE CONFORMING TO ASTM D-3034 SPECIFICATIONS, SDR26 WALL THICKNESS AND ASTM D-3212 GASKET TYPE JOINTS OR ASTM D-2855 SOLVENT WELDED JOINTS; OR P.V.C. PIPE CONFORMING TO ASTM D-3034 SPECIFICATIONS, SDR26 WALL THICKNESS AND ASTM D-3139 JOINT SPECIFICATIONS FOR PRESSURE PIPE; OR CAST IRON PIPE CONFORMING TO ASTM A-74 WITH ASTM C-564 JOINTS.
- 2. ALL WATERMAIN QUALITY PLASTIC PIPE SHALL BE P.V.C. CONFORMING TO NSF STANDARD 14 AND: ASTM STANDARD B 1784 OR AWWA STANDARD C900 OR C905. JOINTING SHALL BE PRESSURE SLIP JOINTED. ELASTOMERIC SEALS (GASKETS) USED FOR PUSH-ON JOINTS SHALL COMPLY WITH ASTM STANDARD F477, AND SHALL BE PRESSURE RATED IN ACCORDANCE WITH ASTM D3139.
- 3. DEFLECTION OF POLYVINYL CHLORIDE (PVC) PIPE SHALL NOT EXCEED 5.0% OF THE "BASE I.D." (INTERNAL DIAMETER) OF THE PIPE. "BASE I.D." SHALL BE CALCULATED IN ACCORDANCE WITH THE FOLLOWING:
AVG ID = AVG OD - 2(1.06)T
TOLERANCE PACKAGE = (A^2 + B^2 + C^2)^(1/2)
WHERE:
A = OD TOLERANCE (ASTM D-3034)
B = EXCESS WALL THICKNESS TOLERANCE = 0.06T
C = OUT-OF-ROUNDNESS TOLERANCE = 0.015 (AVG OD)
T = MINIMUM WALL THICKNESS (ASTM D-3034)
BASE ID = AVG ID - TOLERANCE PACKAGE
DEFLECTION OF COMPOSITE PIPE ("TRUSS" PIPE) SHALL NOT EXCEED 3.0% OF THE AVERAGE INSIDE DIAMETER (ID) OF THE PIPE IN ACCORDANCE WITH ASTM D-2680. THE PIPE LINE SHALL BE TESTED FOR EXCESS DEFLECTING BY PULLING A "GO - NO GO" MANDREL THROUGH THE PIPE FROM MANHOLE TO MANHOLE. THE MANDREL SHALL BE SIZED IN ACCORDANCE WITH SECTION 31-1.11C (4), AND AS SPECIFIED IN THE SPECIAL PROVISIONS. A "DEFLECTOMETER" MAY ALSO BE USED TO CHECK AND RECORD DEFLECTION. WHENEVER POSSIBLE AND PRACTICAL, THE TESTING SHALL INITIATE AT THE DOWNSTREAM LINES AND PROCEED TOWARDS THE UPSTREAM LINES. WHERE THE DEFLECTION IS FOUND TO BE IN EXCESS OF ALLOWABLE TESTING LIMITS, THE CONTRACTOR SHALL EXCAVATE TO THE POINT OF EXCESS DEFLECTION AND CAREFULLY COMPACT AROUND THE POINT WHERE EXCESS DEFLECTION WAS FOUND. THE LINE SHALL THEN BE RETESTED FOR DEFLECTION. HOWEVER, SHOULD AFTER THE INITIAL TESTING THE DEFLECTED PIPE FAIL TO RETURN TO THE ORIGINAL SIZE (INSIDE DIAMETER) THE LINE SHALL BE REPLACED.
- 4. INFILTRATION OR EXFILTRATION SHALL NOT EXCEED 10 GALLONS PER TWENTY-FOUR (24) HOURS PER MILE PER INCH-DIAMETER OF THE SEWER PIPE, FOR ANY SECTION OF THE SYSTEM AND AT ANY TIME DURING ITS SERVICE LIFE. TESTING IS REQUIRED PER THE "STANDARD SPECIFICATIONS FOR WATER AND SEWER MAIN CONSTRUCTION IN MICHIGAN."
- 5. LEAKAGE TESTING FOR MANHOLES FOR WATER TIGHTNESS SHALL BE DONE IN ACCORDANCE WITH ASTM C989-94--"STANDARD PRACTICE FOR INFILTRATION AND EXFILTRATION ACCEPTANCE TESTING OF INSTALLED PRECAST CONCRETE PIPE SEWER LINES", VOL. 04.05, CHEMICAL RESISTANT MATERIALS, VITRIFIED CLAY, CONCRETE, FIBER-CEMENT PRODUCTS; MORTARS; MASONRY (1996)(NO LATER EDITIONS OR AMENDMENTS) OR ASTM C1244-93 "STANDARD TEST METHOD FOR CONCRETE SEWER MANHOLES BY THE NEGATIVE PRESSURE (VACUUM) TEST", VOL. 04.05, CHEMICAL RESISTANT MATERIALS, VITRIFIED CLAY, CONCRETE, FIBER-CEMENT PRODUCTS; MORTARS; MASONRY (1996)(NO LATER EDITIONS OR AMENDMENTS) PRIOR TO PLACING INTO SERVICE.
- 6. ALL STRUCTURE LIDS SHALL BE IMPRINTED "SANITARY".
- 7. ALL WATERTIGHT FRAMES AND LIDS SHALL BE NEENAH R-1916-C. ALL OTHER FRAMES AND LIDS SHALL BE NEENAH R-1550-A WITH A CONCEALED PICK HOLE.
- 8. ALL FRAMES AND LIDS SHALL CONFORM TO ASTM A-48.
- 9. ALL SEWERS ARE TO BE INSTALLED FROM THE DOWNSTREAM END UPSTREAM. IF ANY CONFLICTS ARE ENCOUNTERED, THE DESIGN ENGINEER IS TO BE CONTACTED PRIOR TO THE INSTALLATION OF ANY PIPE.
- 10. FOR A DROP CONNECTION, THE DIAMETER OF THE DROP PIPE SHALL PREFERABLY BE LARGER THAN, OR OF THE SAME DIAMETER AS, THE ENTERING SEWER. THE MINIMUM DIAMETER OF THE DROP PIPE SHALL NOT BE SMALLER THAN THE DIAMETER OF THE ENTERING SEWER BY MORE THAN TWO NOMINAL DIAMETERS, PROVIDED THAT THE MINIMUM DIAMETER OF THE DROP PIPE SHALL NOT BE LESS THAN EIGHT INCHES (8").
- 11. ALL FLOOR DRAINS SHALL DISCHARGE TO THE SANITARY SEWER.

STORM SEWER SPECIFICATIONS

- 1. ALL REINFORCED CONCRETE PIPE SHALL CONFORM TO ASTM C-76 SPECIFICATIONS WITH ASTM C-443 FLAT GASKET JOINTS, OR ASTM C-361 "O-RING" JOINTS WHEN WATER MAIN QUALITY JOINTS ARE REQUIRED.
- 2. ALL PLASTIC PIPE SHALL BE P.V.C. WITH SDR26 WALL THICKNESS AND CONFORM TO D-3034 SPECIFICATIONS WITH ASTM D-3212 GASKET TYPE JOINTS.
- 3. ALL WATERMAIN QUALITY PLASTIC PIPE SHALL BE P.V.C. CONFORMING TO NSF STANDARD 14 AND: ASTM STANDARD B 1784 OR AWWA STANDARD C900 OR C905. JOINTING SHALL BE PRESSURE SLIP JOINTED. ELASTOMERIC SEALS (GASKETS) USED FOR PUSH-ON JOINTS SHALL COMPLY WITH ASTM STANDARD F477, AND SHALL BE PRESSURE RATED IN ACCORDANCE WITH ASTM D3139.
- 4. ALL STRUCTURE LIDS SHALL BE IMPRINTED "STORM".
- 5. ALL FRAMES AND LIDS SHALL CONFORM TO ASTM A-48.
- 6. ALL SEWERS ARE TO BE INSTALLED FROM THE DOWNSTREAM END UPSTREAM. IF ANY CONFLICTS ARE ENCOUNTERED, THE DESIGN ENGINEER IS TO BE CONTACTED PRIOR TO THE INSTALLATION OF ANY PIPE.
- 7. IN PAVED AREAS, ALL FRAMES AND LIDS SHALL BE: NEENAH R-2050 OR R-2502 WITH TYPE D GRATE AT LOW POINTS; OR NEENAH R-3281-A IN CURB AND GUTTER; OR NEENAH R-1550-A WITH SOLID LID, UNLESS OTHERWISE SPECIFIED.
- 8. IN NON-PAVED AREAS, ALL FRAMES AND LIDS SHALL BE: NEENAH R-2090 OR R-2060 WITH TYPE B GRATE AT LOW POINTS; OR NEENAH R-1550-A WITH SOLID LID, UNLESS OTHERWISE SPECIFIED.
- 9. ALL DOWNSPOUTS AND FOOTING DRAINS SHALL DISCHARGE TO THE STORM SEWER.
- 10. ALL FLARED END SECTIONS (FES) ARE TO BE INSTALLED WITH TRASH GRATES.

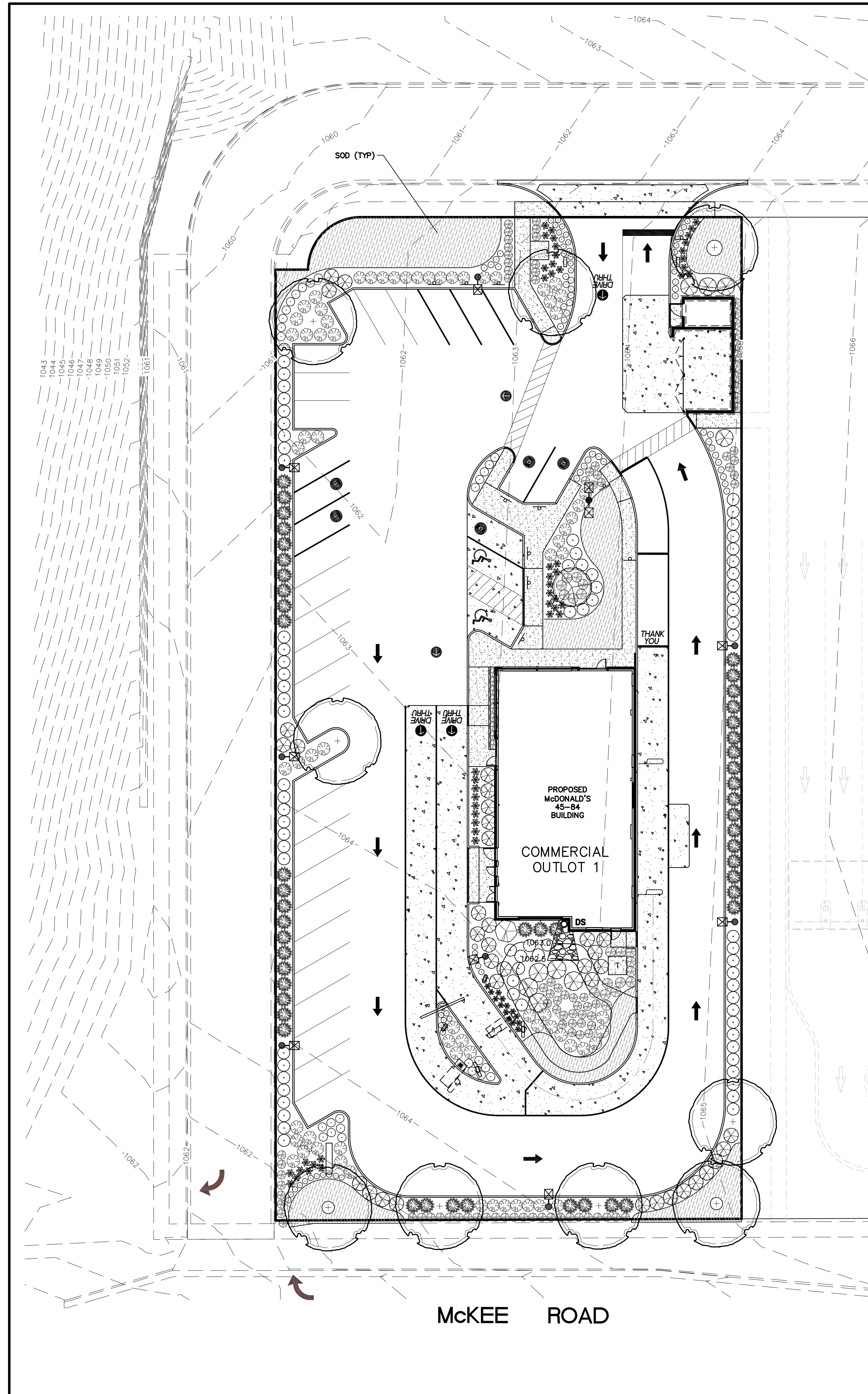
'AMERICANS WITH DISABILITIES ACT' (ADA) MINIMAL REQUIREMENTS:

- 1. GENERAL CONTRACTOR TO BECOME FAMILIAR WITH AND APPLY THE ADA MINIMAL REQUIREMENTS AND REPORT TO ARCHITECT/ENGINEER ANY DISCREPANCIES BEFORE CONSTRUCTION.
- 2. ACCESSIBLE ROUTES ON AN ACCESSIBLE SITE AND FOR ANY NEW SITE IMPROVEMENTS SHALL BE PROVIDED TO SERVE ALL ACCESSIBLE SPACES OR ELEMENTS.
- 3. THE MINIMUM CLEAR WIDTH OF AN ACCESSIBLE ROUTE PER CODE IS 36".
- 4. EACH ACCESSIBLE PARKING SPACE IS TO BE:
4.1. CAR:
A MINIMUM OF 192" WIDE, CONSISTING OF A 96" WIDE ACCESS AISLE AND A 96" WIDE PARKING SPACE, UNLESS OTHERWISE NOTED. (SEE DETAIL). THE ACCESS AISLE SHALL BE PERMITTED TO BE PLACED ON EITHER SIDE OF THE PARKING SPACE. SEE DETAIL FOR REQUIRED DEPTH.
4.2. VAN:
A MINIMUM OF 192" WIDE, CONSISTING OF A 96" WIDE ACCESS AISLE AND A 96" WIDE PARKING SPACE, UNLESS OTHERWISE NOTED (SEE DETAIL). WHEN VAN ACCESSIBLE PARKING SPACES ARE ANGLED, THE ACCESS AISLE SHALL BE LOCATED ON THE PASSENGER SIDE OF THE PARKING SPACE. SEE DETAIL FOR REQUIRED DEPTH.
- 5. ACCESSIBLE PARKING SPACES ARE TO BE LOCATED AS CLOSE TO THE ACCESSIBLE BUILDING ENTRANCE AS POSSIBLE AND SHALL BE IDENTIFIED WITH A SIGN.
- 6. RAMPS MUST NOT EXTEND OUT FROM THE CURB INTO THE ACCESS AISLE OF ANY ACCESSIBLE PARKING SPACE.
- 7. TWO 90 DEGREE ACCESSIBLE PARKING SPACES MAY SHARE AN ACCESS AISLE.
- 8. ACCESS AISLES SHALL BE MARKED SO AS TO DISCOURAGE PARKING IN THEM. (SEE DETAIL)
- 9. ALL ADA PARKING STALLS, ACCESS AISLES AND CROSSWALKS SHALL BE STRIPED USING 4" WIDE DOUBLE LAYER OF HIGH QUALITY YELLOW PAINT, UNLESS OTHERWISE NOTED.
- 10. ACCESSIBLE PARKING SPACES AND ACCESS AISLES SHALL NOT EXCEED A SLOPE OF 1:50 (2.00%) IN ANY DIRECTION.
- 11. EACH ACCESSIBLE PARKING SPACE SHALL HAVE AN IDENTIFICATION SIGN (SEE DETAIL).
- 12. A 24" ELECTRONIC LEVEL WILL BE USED BY MANY INSPECTORS IN THE POST CONSTRUCTION CONDITION TO MEASURE ADA SLOPES. A 24" LEVEL HAS A NARROWER TOLERANCE THAN LONGER ELECTRONIC LEVELS AND REPRESENTS THE WHEELBASE OF A TYPICAL WHEELCHAIR. AS SUCH, A PROPERLY CALIBRATED, 24" ELECTRONIC LEVEL IS RECOMMENDED FOR SETTING AND MEASURING SLOPES IN ALL ADA ACCESSIBLE AREAS THROUGHOUT THE CONSTRUCTION PROCESS.
- 13. AN ACCESSIBLE ROUTE WITH A RUNNING SLOPE GREATER THAN 1:20 (5.00%) IS A RAMP AND SHALL COMPLY WITH THE RAMP REQUIREMENTS.
- 14. AN ACCESSIBLE ROUTE MAY CROSS OPEN PAVEMENT OR FOLLOW A RAMP AS REQUIRED BY SITE-SPECIFIC CONDITIONS. THE RUNNING SLOPE OF AN ACCESSIBLE ROUTE ACROSS OPEN PAVEMENT MUST NOT EXCEED 1:20 (5.00%), WITH A CROSS SLOPE NOT EXCEEDING 1:50 (2.00%). SLOPES EXCEEDING 1:20 (5.00%), BUT LESS THAN 1:12 (8.33%), CONSTITUTE RAMPS AND MUST CONFORM TO THE REQUIREMENTS FOR RAMP DESIGN (HANDRAILS, CURBS, LANDINGS, RISE AND RUN LIMITS, ETC.) AS DETAILED ON THE CIVIL AND ARCHITECTURAL PLANS. NO RAMP SHALL HAVE A RUNNING SLOPE EXCEEDING 1:12 (8.33%), NOR HAVE A CROSS SLOPE EXCEEDING 1:50 (2.00%).
- 15. THE GENERAL CONTRACTOR/CONTRACTOR SHALL MEASURE THE SUBGRADE AND ACROSS FORMS PRIOR TO INSTALLATION OF ASPHALT OR CONCRETE IMPROVEMENTS TO ASSURE THE FINAL IMPROVEMENTS WILL MEET THESE MINIMAL ADA REQUIREMENTS. ANY DISCREPANCIES SHALL BE REPORTED TO THE CIVIL ENGINEER PRIOR TO INSTALLATION OF THE IMPROVEMENTS.

CURB RAMPS

- 16. A CURB RAMP SHALL BE PROVIDED WHEREVER AN ACCESSIBLE ROUTE CROSSES A CURB.
- 17. CURB RAMPS HAVE A MAXIMUM SLOPE OF 1:12 (8.33%) AND DO NOT REQUIRE HANDRAILS.
- 18. IF A CURB RAMP IS LOCATED WHERE PEDESTRIANS MUST WALK ACROSS THE RAMP, OR WHERE IT IS NOT PROTECTED BY HANDRAILS, OR GUARDRAILS, IT SHALL HAVE FLARED SIDES; THE MAXIMUM SLOPE OF THE FLARE SHALL BE 1:12 (8.33%).

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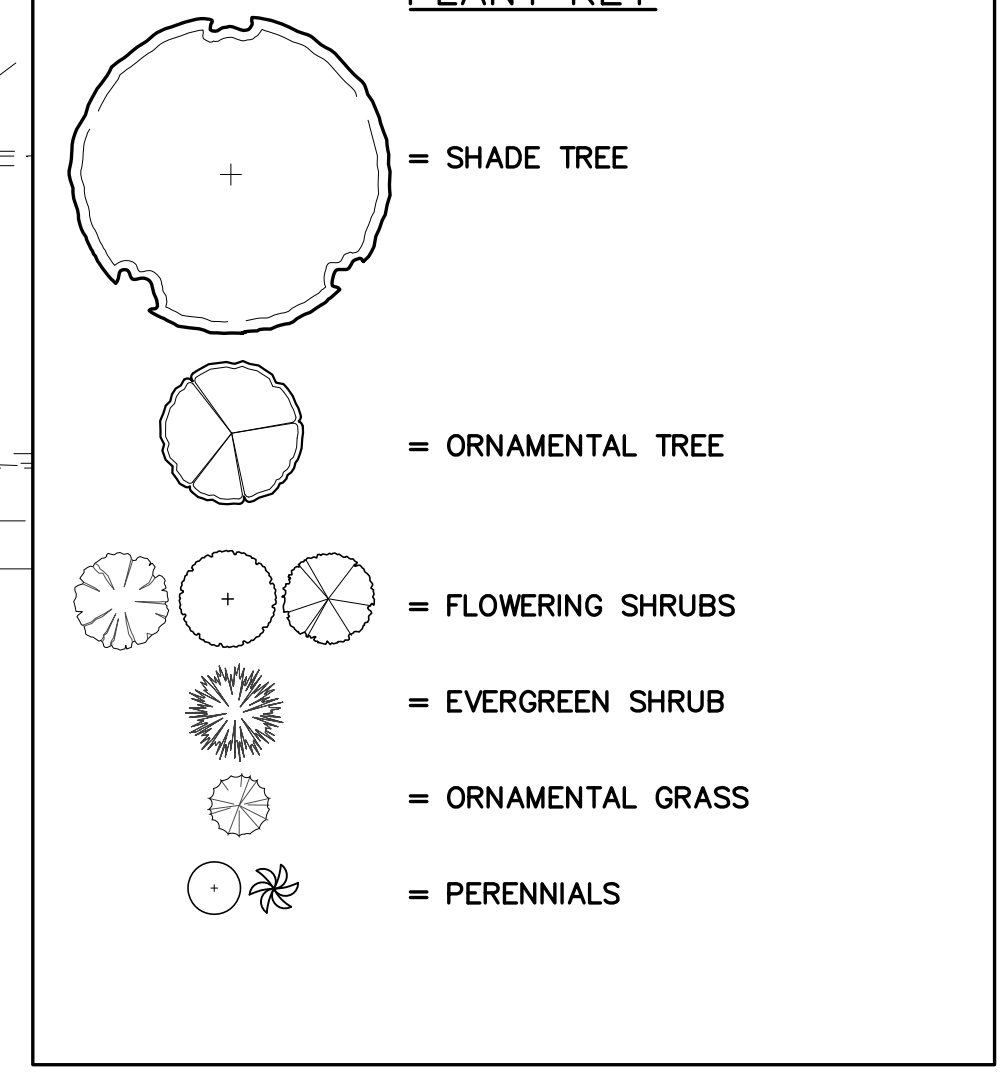
LANDSCAPE NOTES

1. ALL PLANT MATERIAL SHALL BE HARDY TO THE ZONE IT IS BEING PLANTED IN. ALL TREES AND SHRUBS ARE TO BE BALLED AND BURLAPED UNLESS OTHERWISE NOTED AND SHALL BE GROWN IN ACCORDANCE WITH THE STANDARDS SET FORTH BY THE LATEST EDITION OF AMERICAN STANDARD FOR NURSERY STOCK PUBLISHED BY AMERICANHORT.
2. PLANT SIZES CALLED OUT ON THIS PLAN ARE THE MINIMUM SIZE REQUIRED. PLANTS WHICH FAIL TO MEET THE SIZES LISTED, SHALL BE REJECTED AT THE EXPENSE OF THE CONTRACTOR.
3. CONTRACTOR MUST VERIFY ALL MATERIAL QUANTITIES AS DEPICTED ON THE DRAWING. THE PLANT LIST PROVIDED ON THIS PLAN IS FOR CONVENIENCE ONLY.
4. SUBSTITUTIONS MAY NOT BE MADE WITHOUT THE APPROVAL OF THE LANDSCAPE ARCHITECT/DESIGNER.
5. THE CONTRACTOR SHALL NOTIFY ALL APPROPRIATE AGENCIES AND UTILITY LOCATORS PRIOR TO CONSTRUCTION. CONTRACTOR SHALL NOT BEGIN ANY WORK ON-SITE UNTIL ALL UTILITIES HAVE BEEN LOCATED. CONTRACTOR SHALL OBTAIN "AS-BUILT" PLANS FOR ALL IRRIGATION AND LIGHTING PRIOR TO CONSTRUCTION.
6. CONTRACTOR IS RESPONSIBLE FOR PROTECTING ALL UTILITIES INCLUDING IRRIGATION AND LIGHTING. ALL DAMAGE SHALL BE REPAIRED TO A NEW CONDITION IN ACCORDANCE WITH ALL CODES AT NO COST TO THE OWNER - SEE NOTE 5.
7. ALL UNSUITABLE MATERIAL (CONCRETE, AGGREGATE STONE, CRUSHED ASPHALT, BRICK ETC.) SHALL BE REMOVED, INCLUDING HAUL OFF, PRIOR TO PLANTING AND SHALL BE THE RESPONSIBILITY OF THE LANDSCAPE CONTRACTOR.
8. SOIL MIX PM35 BY MIDWEST TRADING COMPANY OR EQUAL SHALL BE ROTOTILLED INTO ALL PERENNIAL AND ANNUAL PLANTING BEDS PRIOR TO THE INSTALLATION OF THE PLANT MATERIAL. A SLOW RELEASE, GRANULAR FERTILIZER SHALL BE APPLIED TO ALL ANNUAL AND PERENNIAL PLANTING BEDS AT THE RECOMMENDED RATE, AND SHALL BE ROTOTILLED IN WITH THE ABOVE SOIL MIXTURE BEFORE THE PLANT MATERIAL IS INSTALLED.
9. CONTRACTOR TO PROVIDE THOROUGH INITIAL WATERING OF ALL PLANTINGS WITHIN 12 HOURS OF INSTALLATION TO ENSURE ALL AIR POCKETS HAVE BEEN REMOVED AROUND ROOT BALL.
10. ALL PLANT BED AREAS ARE TO BE MULCHED WITH 3" OF DOUBLE SHREDDED HARDWOOD MULCH AND SHALL BE SEPARATED WITH A SPADE EDGE ALONG PERIMETERS ADJACENT TO TURF AREAS. FINAL GRADE (AFTER SETTLING) SHALL BE 1" BELOW ADJACENT CURBS.
11. ALL TURF AREAS ARE TO BE A MINIMUM OF A FIVE WAY BLUEGRASS BLEND, UNLESS OTHERWISE NOTED. CONTRACTOR IS RESPONSIBLE FOR WATERING ALL INSTALLED TURF AREAS UNTIL TIME OF KNOTTING. IF TURF SEED AND SOD OCCUR ON THE SAME PROJECT, CONTRACTOR SHALL VERIFY AND USE SEED MIXTURES TO MATCH SOD.
12. AREAS TO BE SODDED SHALL BE WITH AN "APPROVED TURFGRASS SOD" OF PREMIUM GRADE. SOD SHALL BE A 5 WAY BLEND OF IMPROVED KENTUCKY BLUEGRASS VARIETIES THAT HAS BEEN GROWN LOCALLY TO THE PROJECT SITE. SOD MUST BE MATURED FOR 2 FULL GROWING SEASONS PRIOR TO HARVEST CUTTING AND BE HEALTHY WITH WELL ESTABLISHED ROOTS. SOD SHALL BE FREE OF DISEASE, INSECTS AND DEBRIS. SOD SHALL BE UNIFORM IN LEAF COLOR, TEXTURE, AND DENSITY. SOD SHALL BE DELIVERED, INSTALLED, AND WATERED WITHIN 24 HOURS OF HARVEST IN WHICH TEMPERATURES DO NOT EXCEED 90 DEGREES (F) NOR LESS THAN 55 DEGREES (F). SOD SHALL BE MACHINE-CUT AT A MINIMUM UNIFORM SOIL THICKNESS (1.5" OF SOD IS DESIRED) BUT SOD THICKNESS SHALL BE A THICKNESS NECESSARY FOR PLANT VIABILITY. SOD SHALL BE LAID IN STAGGERED STRAIGHT LINES, TIGHTLY AGAINST EACH OTHER WITHOUT STRETCHING OR OVERLAPPING. SOD STAKES SHALL USE ON ALL SLOPES 4:1 OR GREATER.
13. CONTRACTOR SHALL REPAIR ALL DISTURBED AREAS (INTENDED OR UNINTENDED) AT A MINIMUM, TO THE ORIGINAL CONDITION UNLESS OTHERWISE NOTED.
14. THE EXISTING PLANT MATERIAL SHOWN ON THIS PLAN IS INTENDED SOLELY TO IDENTIFY THEM AS OBSERVED IN THE FIELD. THIS PLAN DOES NOT MAKE ANY CLAIMS ABOUT THE CONDITION OR SAFETY OF ANY OF THE PLANT MATERIAL DESCRIBED HEREIN OR OBSERVED IN THE FIELD.
15. ALL TRANSPANTED PLANT MATERIAL MUST BE INSTALLED IMMEDIATELY UPON EXTRACTION FROM ITS ORIGINAL LOCATION, UNLESS SPECIFIC ARRANGEMENTS HAVE BEEN MADE WITH THE LANDSCAPE ARCHITECT/DESIGNER. SHOULD IT BECOME UNREASONABLE TO TRANSPLANT ANY OF THE PLANT MATERIAL AS DESCRIBED IN THIS PLAN, DUE TO SITE CONSTRAINTS OR OTHERWISE, CONTRACTOR IS RESPONSIBLE FOR CONTACTING LANDSCAPE ARCHITECT/DESIGNER TO MAKE ALTERNATIVE ARRANGEMENTS.
16. THE CONTRACTOR SHALL BE RESPONSIBLE FOR MAINTAINING THE HEALTH AND VIABILITY OF THE PROPOSED PLANT MATERIAL INCLUDING WATERING, PROTECTION FROM PHYSICAL DAMAGE FROM THE TIME PLANT IS SELECTED THROUGH ITS INSTALLATION.
17. CONTRACTOR IS RESPONSIBLE FOR ALL PLANT MATERIAL REMAINING PLUMB UNTIL THE END OF THE GUARANTEE PERIOD. PLANTS MAY NOT BE STAKED UNLESS APPROVED BY THE LANDSCAPE ARCHITECT/DESIGNER.
18. CONTRACTOR TO GUARANTEE PLANT MATERIAL AND LABOR FOR A MINIMUM OF ONE YEAR FROM THE TIME OF INSTALLATION.
19. THE CONTRACTOR IS RESPONSIBLE FOR BECOMING FAMILIAR WITH AND ABIDING BY THE LANDSCAPE ORDINANCES FOR THE SPECIFIC JURISDICTION IN WHICH THE WORK IS TAKING PLACE.
20. BIDDERS SHALL BE RESPONSIBLE FOR EXAMINING THE SITE, PRIOR TO PREPARING BID, TO BECOME FAMILIAR WITH THE SPECIFIC SITE CONSTRAINTS.
21. ALL EXISTING ON-SITE PLANT MATERIAL NOT EFFECTED BY CONSTRUCTION OR THE PROPOSED LANDSCAPE, SHALL BE PROTECTED AS PART OF THIS PLAN. EXISTING LANDSCAPE IN AREAS OF CONSTRUCTION AND PROPOSED LANDSCAPE SHALL BE REMOVED AS PART OF THIS PLAN.
22. THE CONTRACTOR SHALL FURNISH ALL LABOR, MATERIALS AND EQUIPMENT NECESSARY FOR THE COMPLETION OF ALL THE ITEMS SHOWN ON THE PLANS.
23. IF IRRIGATION IS DEEMED NECESSARY, THE DESIGN AND INSTALLATION OF THE IRRIGATION SYSTEM SHALL BE THE RESPONSIBILITY OF THE LANDSCAPE CONTRACTOR. AN IRRIGATION PLAN ALONG WITH AN AS BUILT OF THE IRRIGATION SYSTEM SHALL BE PREPARED FOR OWNER REVIEW AND APPROVAL. CONTRACTOR SHALL GUARANTEE PERFORMANCE, PARTS, AND LABOR FOR A PERIOD OF 1 YEAR FROM THE DATE OF FINAL APPROVAL.
24. IF EXISTING IRRIGATION IS PRESENT ON SITE, CONTRACTOR SHALL ADJUST, ADD TO, OR SUBTRACT FROM, THE EXISTING IRRIGATION SYSTEM TO ACCOMMODATE ANY PROPOSED ALTERATIONS/ADDITIONS TO THE EXISTING LANDSCAPE. CONTRACTOR SHALL PROVIDE THE OWNER AN AS BUILT OF THE IRRIGATION SYSTEM AND ALL CHANGES TO THE SYSTEM AFFECTED BY THIS PROJECT.
25. DURING THE BIDDING PROCESS, THE LANDSCAPE CONTRACTOR SHALL COORDINATE WITH THE GENERAL CONTRACTOR OR OTHER PARTIES RESPONSIBLE FOR THE OVERALL BIDDING OF THE PROJECT TO DETERMINE WHICH CONTRACTOR SHALL BE RESPONSIBLE FOR THE REQUIRED TOPSOIL RE-SPREAD PER THE FOLLOWING UNLESS OTHERWISE NOTED:
 - A. 4" MINIMUM IN GRASS OR SOD AREAS
 - B. 6" MINIMUM IN PLANTING AREAS
 - C. 12" MINIMUM IN LANDSCAPE ISLANDS

PLANT LIST

SHADE, ORNAMENTAL, AND CONIFEROUS TREES			
QTY	ABRV	BOTANICAL NAME	COMMON NAME
2	SSM	Acer miyabei 'Morton'	State Street Maple
2	MAM	Acer x fremanii 'Marmo'	Marmo Maple
5	FTE	Ulmus x frontier	Frontier Elm
1	ABS	Amelanchier x grandiflora 'Autumn Brilliance'	Autumn Brilliance Serviceberry
FLOWERING AND EVERGREEN SHRUBS			
QTY	ABRV	BOTANICAL NAME	COMMON NAME
45	ALH	Aronia melanocarpa 'UCONNAM165'	Aronia Low Scape Hedger
77	KOH	Dierilla 'G2X8544'	Kodiak Orange Honeysuckle
1	VWH	Hamamelis vernalis	Vernal Witchhazel
7	BOH	Hydrangea paniculata 'Bobo'	Bobo Hydrangea
26	LQH	Hydrangea paniculata 'SMHPLQ'	Little Quick Fire Hydrangea
5	GLS	Rhus aromatica 'Grow-Low'	Grow-Low Sumac
4	MKL	Syringa patula 'Miss Kim'	Miss Kim Lilac
30	DHW	Weigela x 'Dark Horse'	Dark Horse Weigela
49	DNY	Taxus x media 'Densiformis'	Densiformis Yew
PERENNIALS, ORNAMENTAL GRASS AND GROUNDCOVERS			
QTY	ABRV	BOTANICAL NAME	COMMON NAME
18	SBA	Allium 'Summer Beauty'	Summer Beauty Onion
13	BCC	Gampanula carpatia 'Blue Clips'	Blue Clips Bellflower
45	FWD	Dianthus gratianopolitanus 'Firewitch'	Firewitch Dianthus
35	ASD	Hemerocallis x 'Apricot Sparkles'	Apricot Sparkles Daylily
27	WLC	Nepeta racemosa 'Walker's Low'	Walker's Low Catmint
32	DTB	Penstemon digitalis 'Dark Towers'	Dark Towers Beardtongue
30	KFF	Calamagrostis x acutiflora 'Karl Foerster'	Karl Foerster Feather Reed Grass
91	PDS	Sporobolus heterolepis	Prairie Dropseed

PLANT KEY



GENERAL NOTES:
 1. THESE PLANS ARE BASED ON THE FINAL ENGINEERING PLANS (PROJECT #22-11636 DATED 9/11/24)
 PREPARED BY: JSD
 507 W. VERONA AVE., SUITE 500, VERONA, WI 53593
 2. PRIOR TO CONSTRUCTION, CONTRACTOR TO CONTACT THE DESIGN ENGINEER AND ARCHITECT TO VERIFY THAT THEY ARE WORKING FROM THE MOST CURRENT SET OF PLANS AND SPECIFICATIONS.

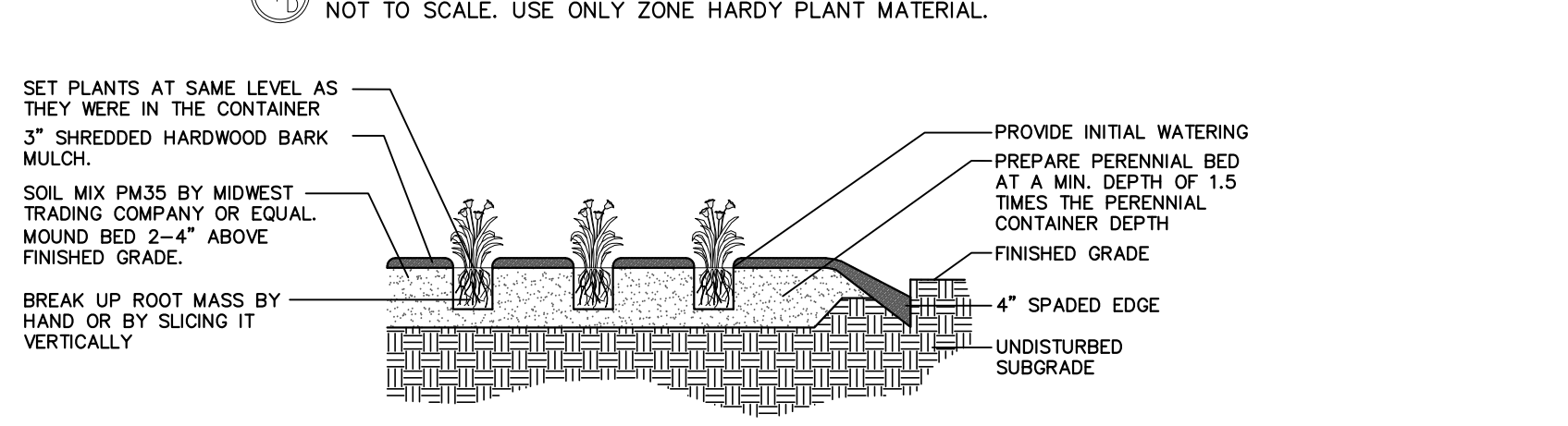
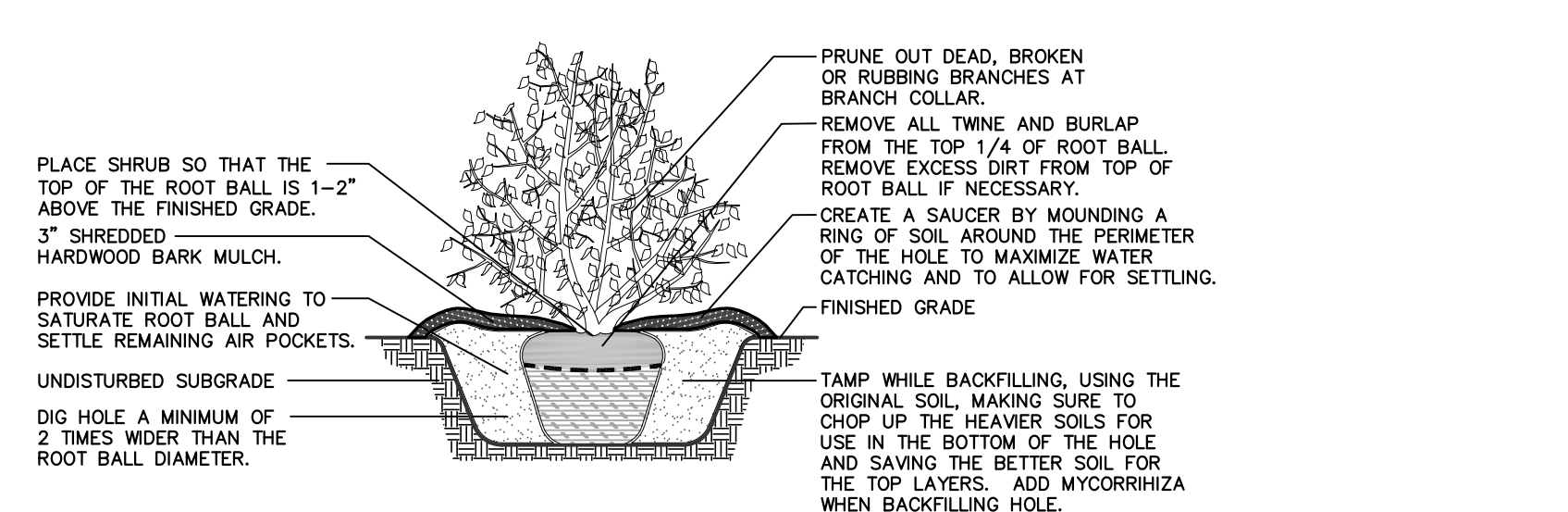
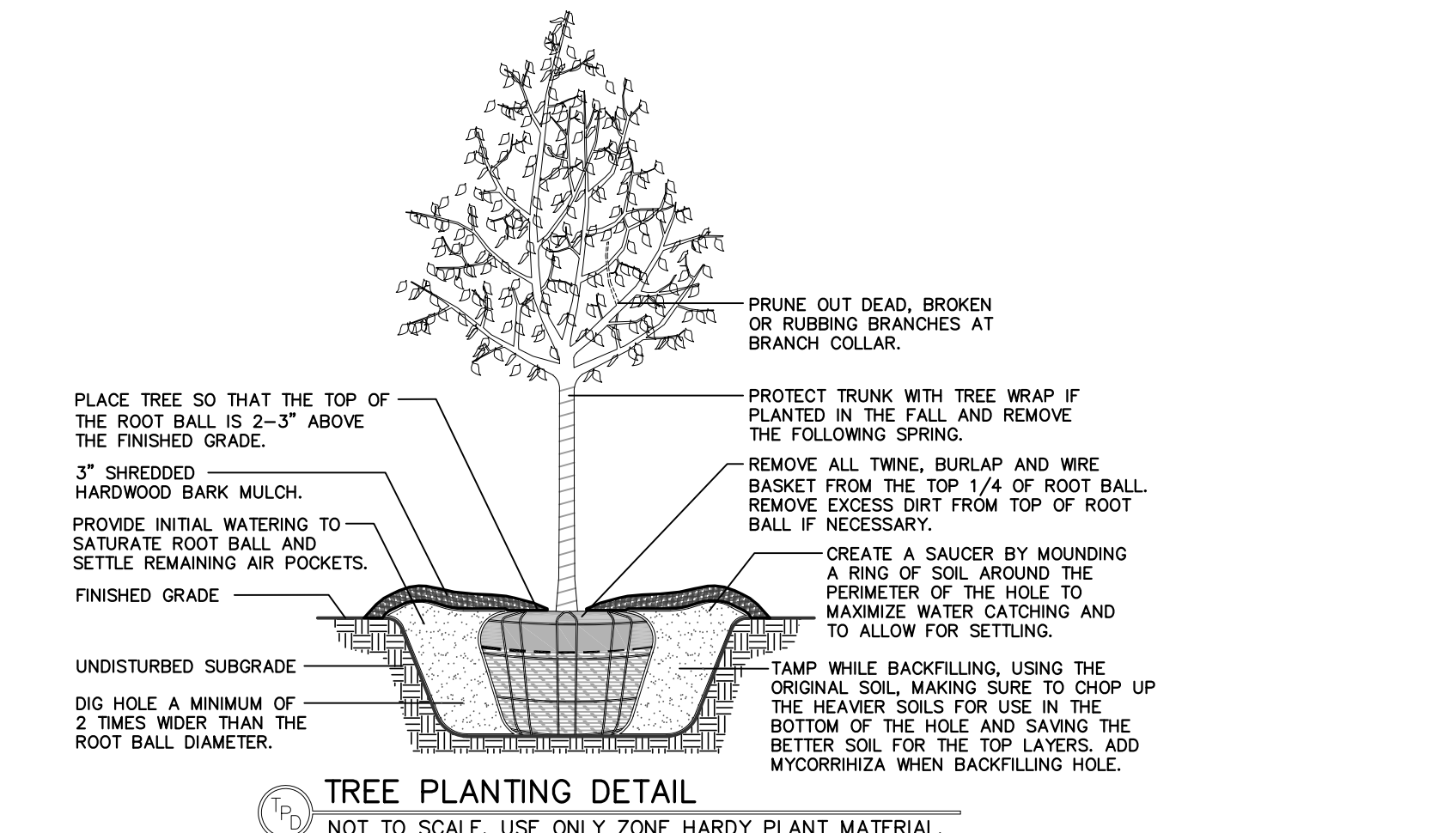
ON SITE PARKING DATA

REGULAR SPACES	32
ADA ACCESSIBLE SPACES	2
TOTAL SPACES	34

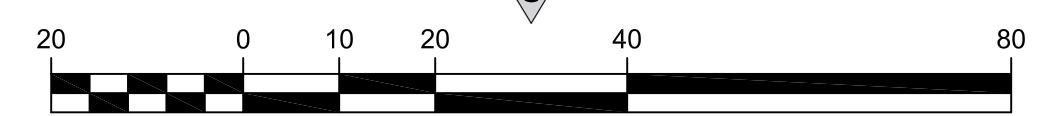
PARKING REQUIREMENT: 6 SPACES PER 1000 SF OF GROSS BUILDING AREA (3,900/1000*6 = 23 SPACES REQUIRED)
 34 SPACES PROVIDED > 23 SPACES REQUIRED ∴ OK

SITE DATA

ZONING	= PDD-GIP
LOT AREA	= 48,710 S.F. (1.12 AC.)
PERVIOUS AREA	= 12,137 S.F. (25%)
IMPERVIOUS AREA	= 36,573 S.F. (75%)
BUILDING AREA	= 3,900 S.F.±
SEATS	= 35±



Know what's Below.
Call before you dig.



LANDSCAPE PLAN

DATE: 10/21/25
 NO. 1
 REVISIONS: PEER ARCHITECTURAL DESIGN COMMENTS & IN-HOUSE COORDINATION

Prepared For:
McDonald's
 110 N. Carpenter St.
 Chicago, IL 60607

McDONALD'S - FITCHBURG, WI
 NWC of McKee Road and Fitchrona Road
 Fitchburg, Wisconsin

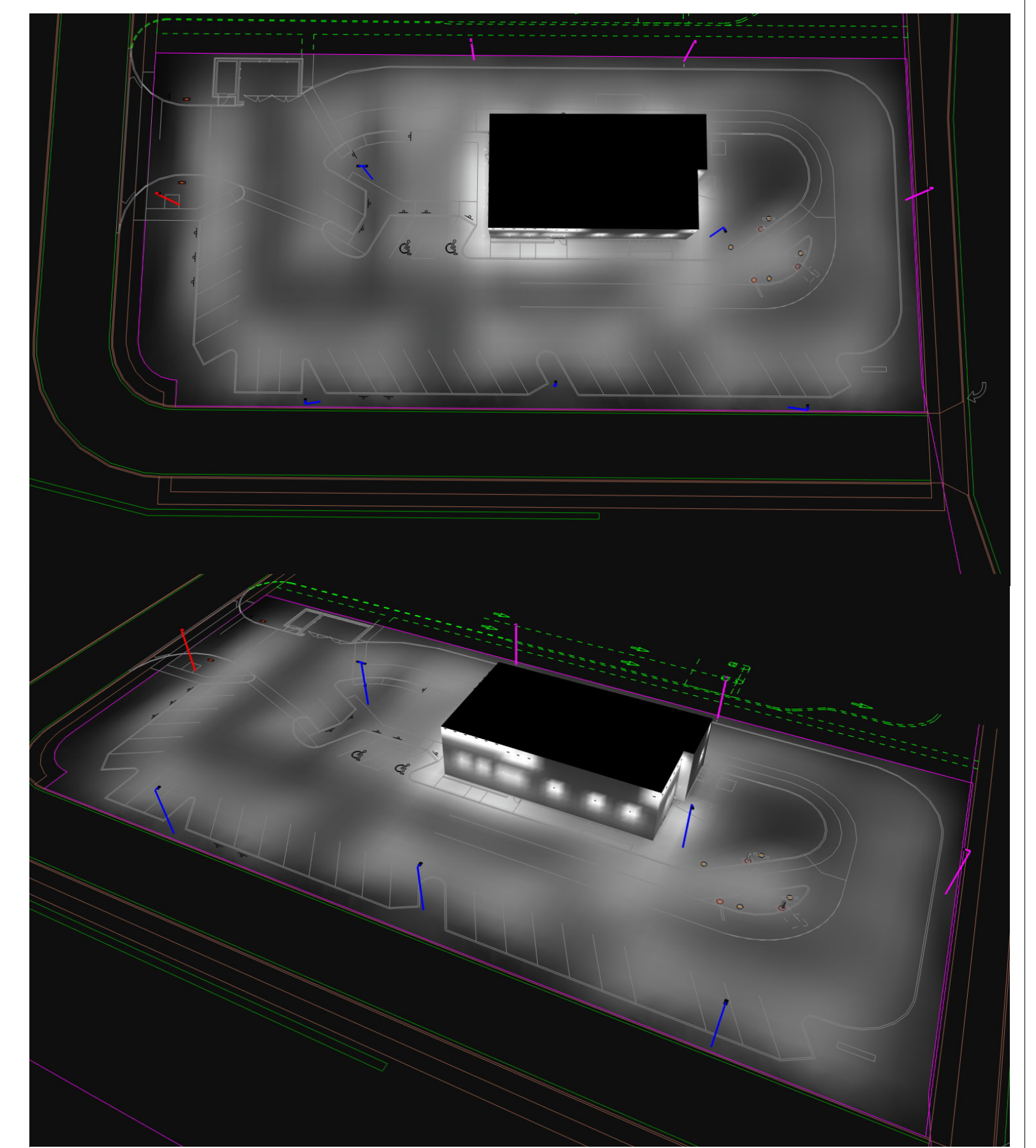
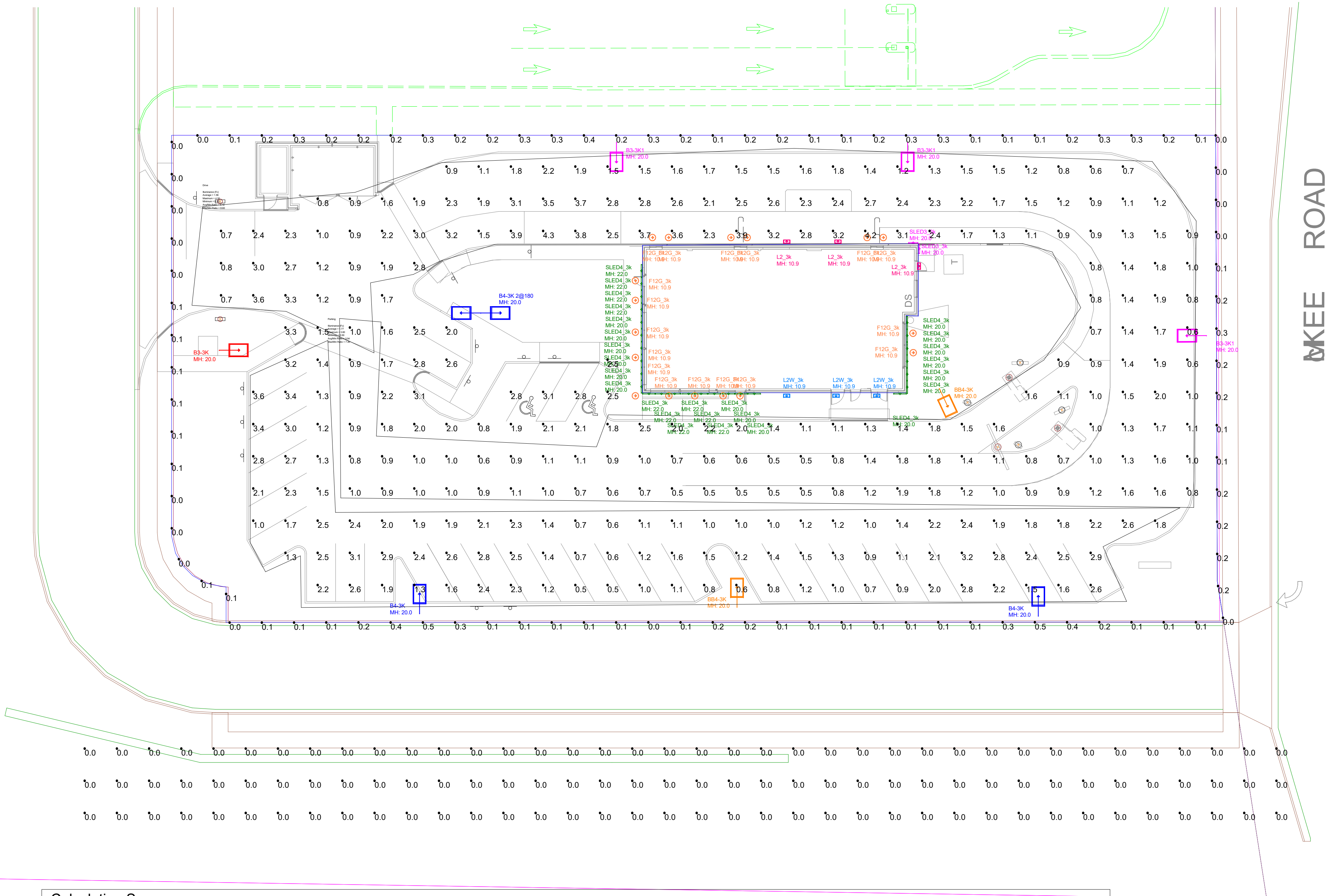
Prepared By:
Watermark Engineering Resources
 watermark-engineering.com | 2631 Ginger Woods Pkwy | Aurora, IL 60502 | (630) 375-1800

CHECKED BY: J. MILLER
 DESIGN BY: J. VOLANTI
 DRAWN BY: J. VOLANTI
 DATE: DECEMBER 3, 2024
 SCALE: 1" = 20'
 PROJECT NO.: 24-001

L-1
 LC #48-1082

LANDSCAPE PLAN

NOTES:
 1. THE FOOTCANDLE LEVELS AS SHOWN ARE BASED ON THE FOLLOWING CRITERIA. ANY SUBSTITUTIONS IN SPECIFIED FIXTURES OR CHANGES TO LAYOUT WILL AFFECT LIGHTING LEVELS SHOWN AND WILL NOT BE THE RESPONSIBILITY OF SECURITY LIGHTING.
 2. DISTANCE BETWEEN READINGS _____ 10'



Pole Fixtures Are Full Cutoff
 Tilt=0
 Calculation Grids Are At Grade
 Pole Light Mounting Height=20ft
 (17' Pole + 3' Base)


Parking Max Avg 2.5FC (1.77FC Avg)
 Drive Max Avg 1.5FC (1.56FC Avg)
 Max Propertyline 0.5FC
 Max Pole Height 20'

Calculation Summary							
Label	CalcType	Units	Avg	Max	Min	Avg/Min	Max/Min
Pave Surface Areas	Illuminance	Fc	1.68	4.3	0.5	3.36	8.60
Propertyline	Illuminance	Fc	0.15	0.5	0.0	N.A.	N.A.
Residential Property	Illuminance	Fc	0.00	0.0	0.0	N.A.	N.A.
Drive	Illuminance	Fc	1.56	4.30	0.50	3.12	8.60
Parking	Illuminance	Fc	1.77	3.60	0.50	3.54	7.20

Luminaire Schedule									
Symbol	Qty	Label	Arrangement	LLF	Description	Lum. Watts	EPA	Mtg Height	Pole Type
[Symbol]	1	B3-3K	Single	0.800	RAR-1-160L-115-3K7-3-BC	109.7	0.607	20	SES-17-40-1-TA-GL-xx (4")
[Symbol]	3	B3-3K1	Single	0.850	RAR-1-160L-70-3K7-3-BC	68.4	0.607	20	SES-17-40-1-TA-GL-xx (4")
[Symbol]	2	B4-3K	Single	0.800	RAR-1-160L-115-3K7-4W-BC	109.7	0.607	20	SES-17-40-1-TA-GL-xx (4")
[Symbol]	1	B4-3K 2@180	Back-Back	0.800	RAR-1-160L-115-3K7-4W-BC	109.7	1.214	20	SES-17-40-1-TA-GL-xx (4")
[Symbol]	2	BB4-3K	Single	0.800	RAR-1-80L-50-3K7-4W-BC	49.8	0.607	20	SES-17-40-1-TA-GL-xx (4")
[Symbol]	17	F12G_3k	SINGLE	0.250	LBRA-6RD-T-10LCS9-WH_30K	13.318			
[Symbol]	3	L2W_3k	SINGLE	0.250	RWSC-36L-3K-DO-U-WH	14.4			
[Symbol]	3	L2_3k	SINGLE	0.260	RWSC-36L-3K-DO-U-PS	14.4			
[Symbol]	2	SLED3_3k	SINGLE	0.250	SLED-HE-36-DO-3K-UNV	14.1			

THIS DRAWING MEETS OR EXCEEDS MCDONALD'S CURRENT ILLUMINATION SPECIFICATIONS OF A 3-4 FOOTCANDLE AVERAGE, UNLESS SUPERSEDED BY OTHER REQUIREMENTS.

PROJECT WIND LOAD CRITERIA BASED ON:
 ASCE 7-10 WIND SPEEDS (3-SEC PEAK GUST MPH)
 50 YEAR MEAN RECURRENCE INTERVAL
 ALLOWED EPA 13.6 @ WIND LOAD 90 MPH



Regional Drawing
48-1082

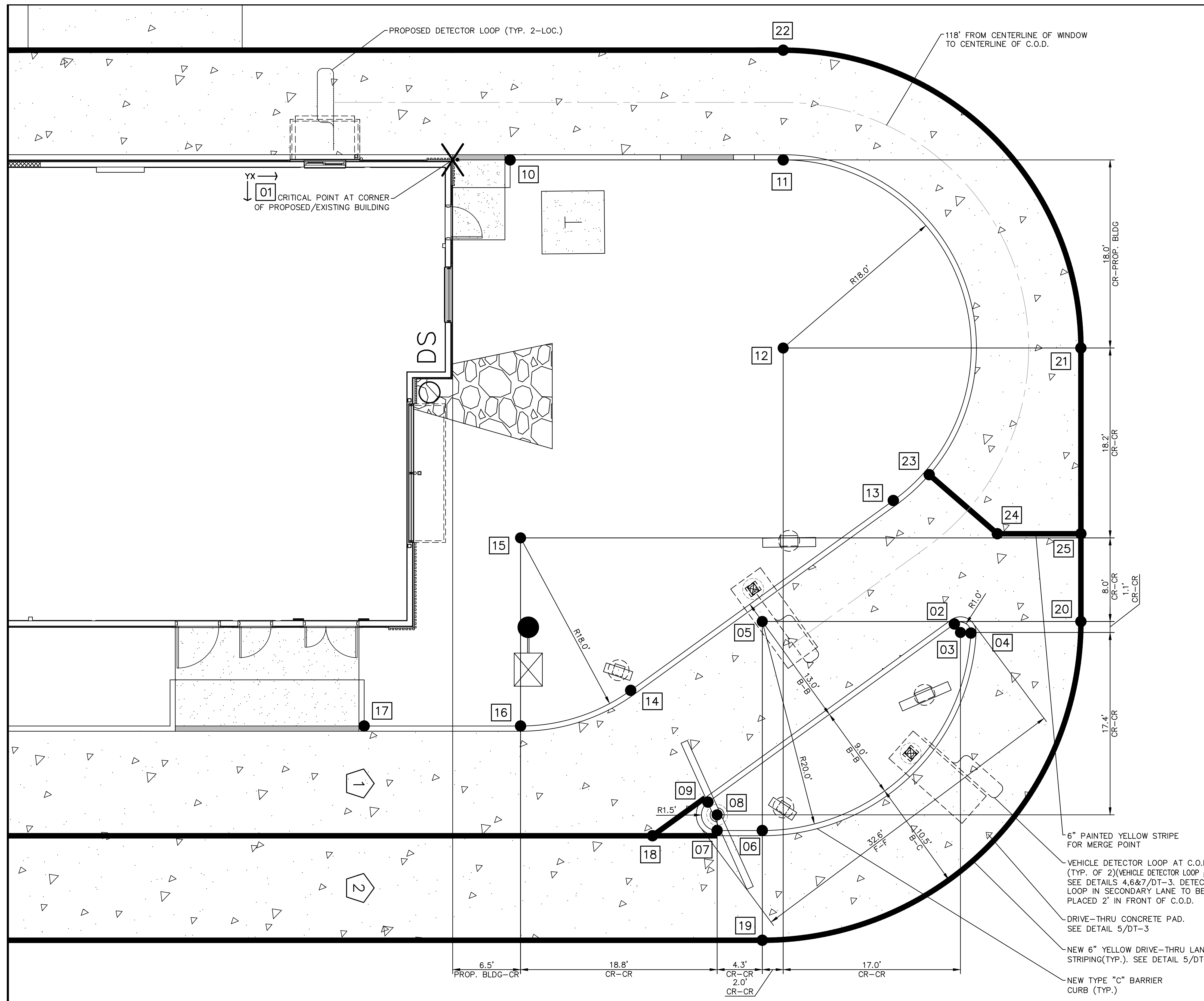
UNLESS OTHERWISE SPECIFIED, ALL DIMENSIONS ARE IN INCHES
 SCALE 1"=20' 0"
 DRAWN BY: CLB

POINT-BY-POINT FOOTCANDLE PLOT FOR
MCDONALD'S
Fitchburg, WI

NATIONAL STORE NUMBER
43262

DATE: 1/9/2025
DRAWING NUMBER: A242316E.AGI

1. THIS LIGHTING DESIGN IS BASED ON INFORMATION SUPPLIED BY OTHERS TO SECURITY LIGHTING SYSTEMS. SITE DETAILS PROVIDED HEREON ARE REPRODUCED ONLY AS A VISUALIZATION AID. FIELD DEVIATIONS MAY SIGNIFICANTLY AFFECT PREDICTED PERFORMANCE. PRIOR TO INSTALLATION, CRITICAL SITE INFORMATION (POLE LOCATIONS, ORIENTATION, MOUNTING HEIGHT, ETC.) SHOULD BE COORDINATED WITH THE CONTRACTOR AND/OR SPECIFIER RESPONSIBLE FOR THE PROJECT.
 2. LUMINAIRE DATA IS TESTED TO INDUSTRY STANDARDS UNDER LABORATORY CONDITIONS. OPERATING VOLTAGE AND NORMAL MANUFACTURING TOLERANCES OF LAMP, BALLAST, AND LUMINAIRE MAY AFFECT FIELD RESULTS.
 3. CONFORMANCE TO FACILITY CODE AND OTHER LOCAL REQUIREMENTS IS THE RESPONSIBILITY OF THE OWNER AND/OR THE OWNER'S REPRESENTATIVE.
 4. THIS LAYOUT MAY NOT MEET TITLE 24 OR LOCAL ENERGY REQUIREMENTS. IF THIS LAYOUT NEEDS TO BE COMPLIANT WITH TITLE 24 OR OTHER ENERGY REQUIREMENTS, PLEASE CONSULT FACTORY WITH SPECIFIC DETAILS REGARDING PROJECT REQUIREMENTS SO THAT REVISIONS MAY BE MADE TO THE DRAWING.



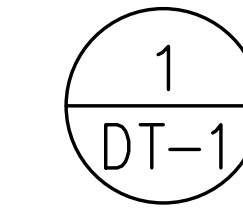
GENERAL NOTES:

1. THE REGIONAL CONSTRUCTION MANAGER IS TO REVIEW AND APPROVE ALL DRIVE-THRU LAYOUTS. A DRIVE-THRU IS FINAL, AND CONSIDERED "RED", ONCE APPROVED. NO CHANGES ARE TO BE MADE AFTER THIS POINT.
2. DUE TO THE EXACT GEOMETRY REQUIRED FOR THE EFFICIENT OPERATION OF THIS DRIVE-THRU LAYOUT, IT IS RECOMMENDED THAT ALL DRIVE-THRU EQUIPMENT AND PAVEMENT IMPROVEMENTS TO BE FIELD LOCATED BY A LICENSED SURVEYOR.
3. THE PLACEMENT OF THE CODs AND ANY ADDITIONAL SIGNAGE SHOULD BE SUCH THAT IT PREVENTS, OR MINIMIZES, BLOCKING THE CUSTOMER'S VIEW OF THE MENU BOARD WHILE ORDERING.
4. THESE DIMENSIONS ARE CRITICAL TO THE FUNDAMENTAL LAYOUT OF THE SIDE BY SIDE DESIGN.
5. IF DIMENSIONS ARE MODIFIED CONTACT DESIGN ENGINEER IMMEDIATELY.
6. VERIFY WITH SUPPLIER OF DRIVE-THRU EQUIPMENT THAT MOST CURRENT EQUIPMENT IS BEING UTILIZED.

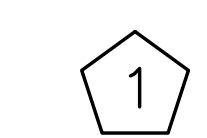
COORDINATES

Pavement		X		Y		DESCRIPTION
1	0.00'	0.00'	0.00'	0.00'	0.00'	CRITICAL STARTING POINT FOR ALL COORDINATES
2	48.03'	44.41'	44.41'	44.41'	44.41'	TO BACK OF CURB AT START OF RADIUS
3	48.61'	45.22'	45.22'	45.22'	45.22'	TO CENTER OF RADIUS (1.00')
4	49.61'	45.27'	45.27'	45.27'	45.27'	TO BACK OF CURB AT END OF RADIUS
5	29.64'	44.17'	44.17'	44.17'	44.17'	TO CENTER OF RADIUS (20.00')
6	29.64'	64.17'	64.17'	64.17'	64.17'	TO BACK OF CURB AT START OF RADIUS
7	25.32'	64.17'	64.17'	64.17'	64.17'	TO BACK OF CURB AT END OF RADIUS
8	25.32'	62.67'	62.67'	62.67'	62.67'	TO CENTER OF RADIUS (1.50')
9	24.44'	61.45'	61.45'	61.45'	61.45'	TO BACK OF CURB AT START OF RADIUS
10	5.50'	0.00'	0.00'	0.00'	0.00'	TO BACK OF CURB AT START OF RADIUS
11	31.64'	0.00'	0.00'	0.00'	0.00'	TO BACK OF CURB AT END OF RADIUS
12	31.64'	18.00'	18.00'	18.00'	18.00'	TO CENTER OF RADIUS (18.00')
13	42.18'	32.59'	32.59'	32.59'	32.59'	TO BACK OF CURB AT START OF RADIUS
14	17.04'	50.76'	50.76'	50.76'	50.76'	TO BACK OF CURB AT END OF RADIUS
15	6.50'	36.17'	36.17'	36.17'	36.17'	TO CENTER OF RADIUS (18.00')
16	6.50'	54.17'	54.17'	54.17'	54.17'	TO BACK OF CURB AT START OF RADIUS
17	-8.47'	54.17'	54.17'	54.17'	54.17'	TO BACK OF CURB AT END OF RADIUS
18	19.13'	64.67'	64.67'	64.67'	64.67'	TO STRIPING/CONCRETE
19	29.64'	74.67'	74.67'	74.67'	74.67'	TO STRIPING/CONCRETE
20	60.14'	44.17'	44.17'	44.17'	44.17'	TO STRIPING/CONCRETE
21	60.14'	18.00'	18.00'	18.00'	18.00'	TO STRIPING/CONCRETE
22	31.64'	-10.50'	-10.50'	-10.50'	-10.50'	TO STRIPING/CONCRETE
23	45.62'	30.12'	30.12'	30.12'	30.12'	TO MERGE POINT STRIPING
24	52.14'	35.77'	35.77'	35.77'	35.77'	TO MERGE POINT STRIPING
25	60.14'	35.77'	35.77'	35.77'	35.77'	TO MERGE POINT STRIPING

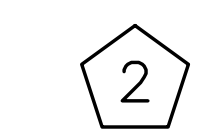
DRIVE-THRU APPROACH



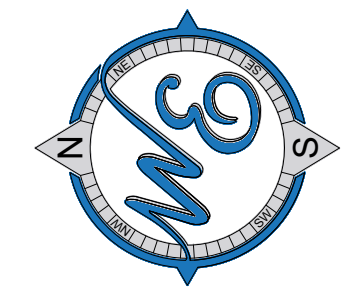
1" = 5'-0"



PRIMARY DRIVE-THRU LANE



SECONDARY DRIVE-THRU LANE



DRIVE-THRU PAVEMENT PLAN

DATE: _____

NO. _____

REVISIONS: _____

Prepared For: _____

McDonald's
110 N. Carpenter St.
Chicago, IL 60607

McDonald's - FITCHBURG, WI
NWC of McKee Road and Fitchrona Road
Fitchburg, Wisconsin

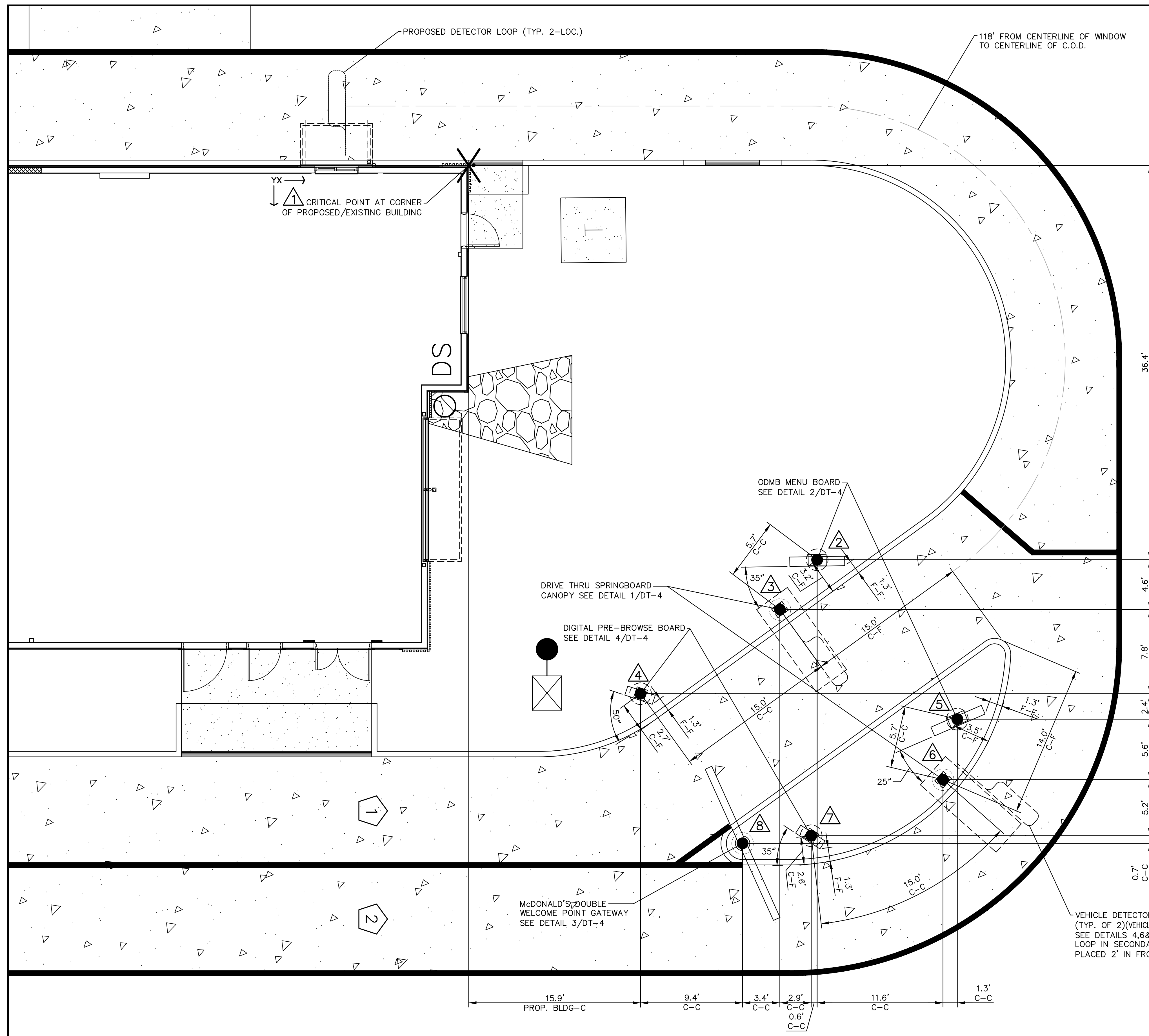
Prepared By: _____

Watermark Engineering Resources
watermark-engineering.com | 2631 Ginger Woods Pkwy | Aurora, IL 60502 | (630) 975-1800

CHECKED BY: J. MILLER
DESIGN BY: D. OLSON
DRAWN BY: JOSH MILLER
DATE: DECEMBER 3, 2024
SCALE: 1" = 5'
PROJECT NO.: 24-001

DT-1
LC #48-1082

DRIVE-THRU PAVEMENT PLAN

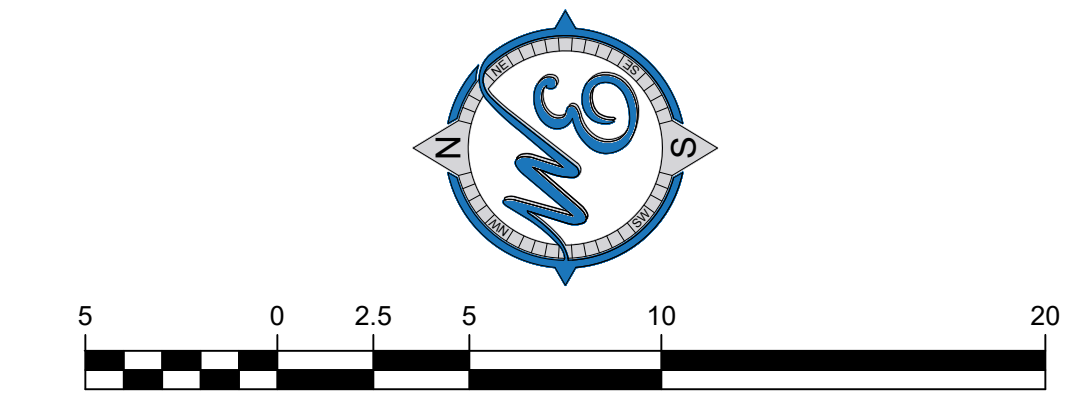


- GENERAL NOTES:**
1. THE LOCATION AND ORIENTATION OF MENU BOARDS WERE DETERMINED BY THE SIGHT LINES OF THE CARS. THE CARS WERE POSITIONED ACCORDING TO THE MAX. AMOUNT OF CARS DURING A PEAK PERIOD. ALL DIMENSIONS SHOWN ARE BASED UPON PROTOTYPICAL LAYOUTS SHOWN. ACTUAL DIMENSIONS TO BE BASED ON SITE SPECIFIC CONFIGURATIONS AND THE REQUIREMENTS OF NOTE 2.
 2. THE MEASUREMENT GIVEN FROM THE CENTER OF THE SPRINGBOARD CANOPY TO THE CENTER OF THE PRE-BROWSE BOARD IS MEASURED ALONG THE FACE OF CURB.
 3. PROVIDE TWO CIRCUITS FOR MENU BOARD AND PRE-VIEW BOARDS. ONE CIRCUIT FOR LIGHTS AND ONE CIRCUIT WITH LOCK FOR MOTOR AND CONTROLLER. REFER TO ARCHITECTURAL/ELECTRICAL PLANS FOR TERMINATION OF CONDUIT RUNS INSIDE BUILDING.
 4. ALL GALVANIZED ANCHOR BOLTS TO BE SUPPLIED AND INSTALLED BY THE CONTRACTOR.
 5. PLACEMENT NOTE: THE PLACEMENT OF THE CUSTOMER ORDER DISPLAY SHOULD BE SUCH THAT IT DOES NOT BLOCK THE VIEWING OF THE MENU BOARD FROM THE DRIVER'S VANTAGE POINT.
 6. VERIFICATION NOTE: VERIFY ALL DRIVE-THRU EQUIPMENT WITH McDONALD'S PROJECT MANAGER AND OPERATIONS MANAGER BEFORE PROCEEDING.
 7. THE REGIONAL CONSTRUCTION MANAGER IS TO REVIEW AND APPROVE ALL DRIVE-THRU LAYOUTS. A DRIVE-THRU IS FINAL, AND CONSIDERED "RED", ONCE APPROVED. NO CHANGES ARE TO BE MADE AFTER THIS POINT.
 8. DUE TO THE EXACT GEOMETRY REQUIRED FOR THE EFFICIENT OPERATION OF THIS DRIVE-THRU LAYOUT, IT IS RECOMMENDED THAT ALL DRIVE-THRU EQUIPMENT AND PAVEMENT IMPROVEMENTS TO BE FIELD LOCATED BY A LICENSED SURVEYOR.
 9. THE PLACEMENT OF THE CODs AND ANY ADDITIONAL SIGNAGE SHOULD BE SUCH THAT IT PREVENTS, OR MINIMIZES, BLOCKING THE CUSTOMER'S VIEW OF THE MENU BOARD WHILE ORDERING.
 10. THESE DIMENSIONS ARE CRITICAL TO THE FUNDAMENTAL LAYOUT OF THE SIDE BY SIDE DESIGN.
 11. IF DIMENSIONS ARE MODIFIED CONTACT DESIGN ENGINEER IMMEDIATELY.
 12. VERIFY WITH SUPPLIER OF DRIVE-THRU EQUIPMENT THAT MOST CURRENT EQUIPMENT IS BEING UTILIZED.

COORDINATES

Equipment	X	Y	DESCRIPTION
1	0.00'	0.00'	CRITICAL STARTING POINT FOR ALL COORDINATES
2	32.21'	36.45'	TO CENTER OF ODMB MENU BOARD
3	28.76'	41.05'	TO CENTER OF DRIVE THRU SPRINGBOARD CANOPY
4	15.88'	48.83'	TO CENTER OF DIGITAL PRE-BROWSE BOARD
5	45.17'	51.19'	TO CENTER OF ODMB MENU BOARD
6	43.85'	56.78'	TO CENTER OF DRIVE THRU SPRINGBOARD CANOPY
7	31.66'	61.96'	TO CENTER OF DIGITAL PRE-BROWSE BOARD
8	25.32'	62.67'	TO CENTER OF FOUNDATION OF DOUBLE WELCOME POINT GATEWAY

- DRIVE-THRU APPROACH**
- 1" = 5'-0"
- ① PRIMARY DRIVE-THRU LANE
 - ② SECONDARY DRIVE-THRU LANE



DRIVE-THRU EQUIPMENT PLAN

DATE: _____

NO. _____

REVISIONS: _____

Prepared For: _____

McDonald's
110 N. Carpenter St.
Chicago, IL 60607

McDONALD'S - FITCHBURG, WI
NWC of McKee Road and Fitchrona Road
Fitchburg, Wisconsin

Prepared By: _____

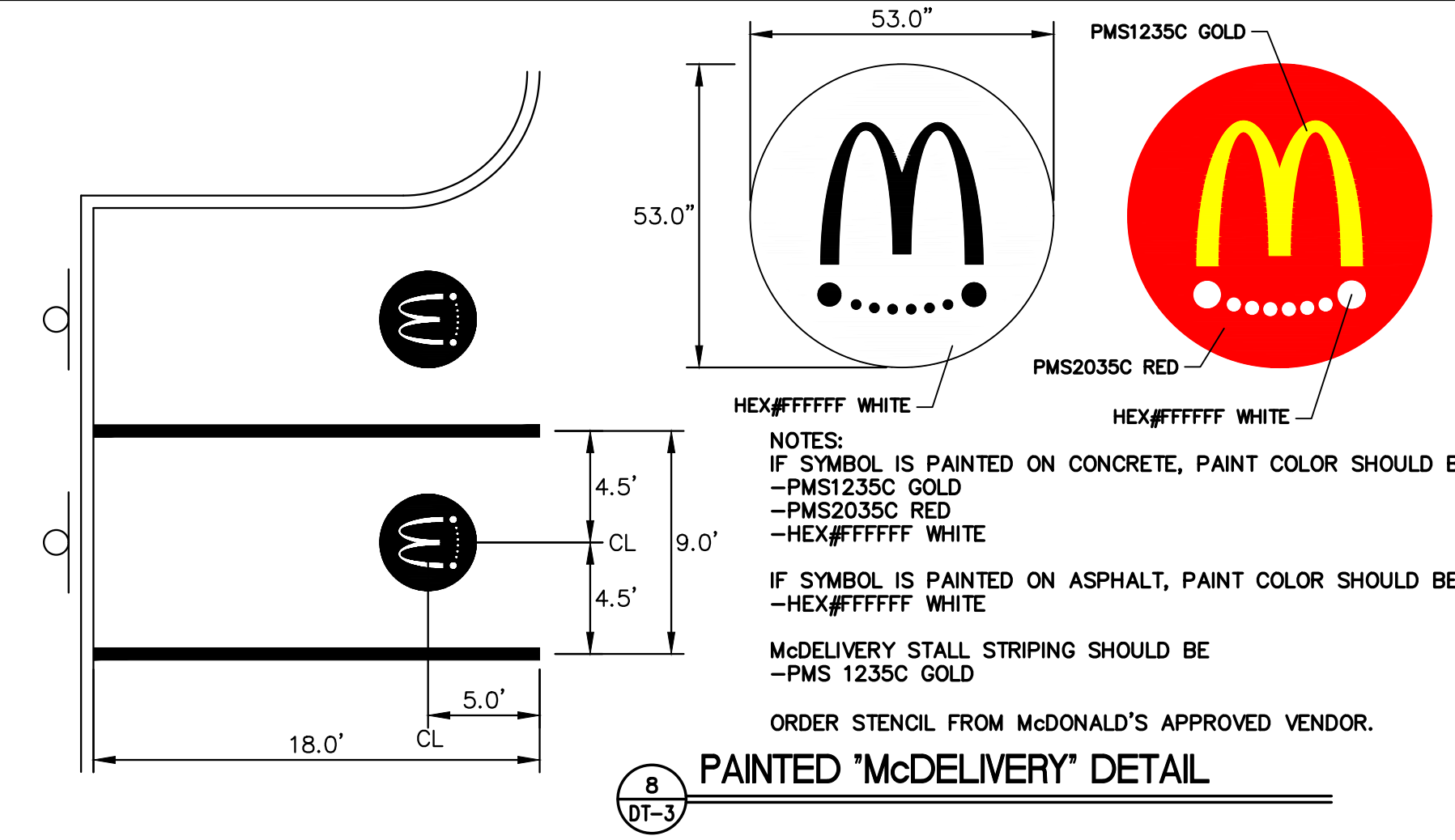
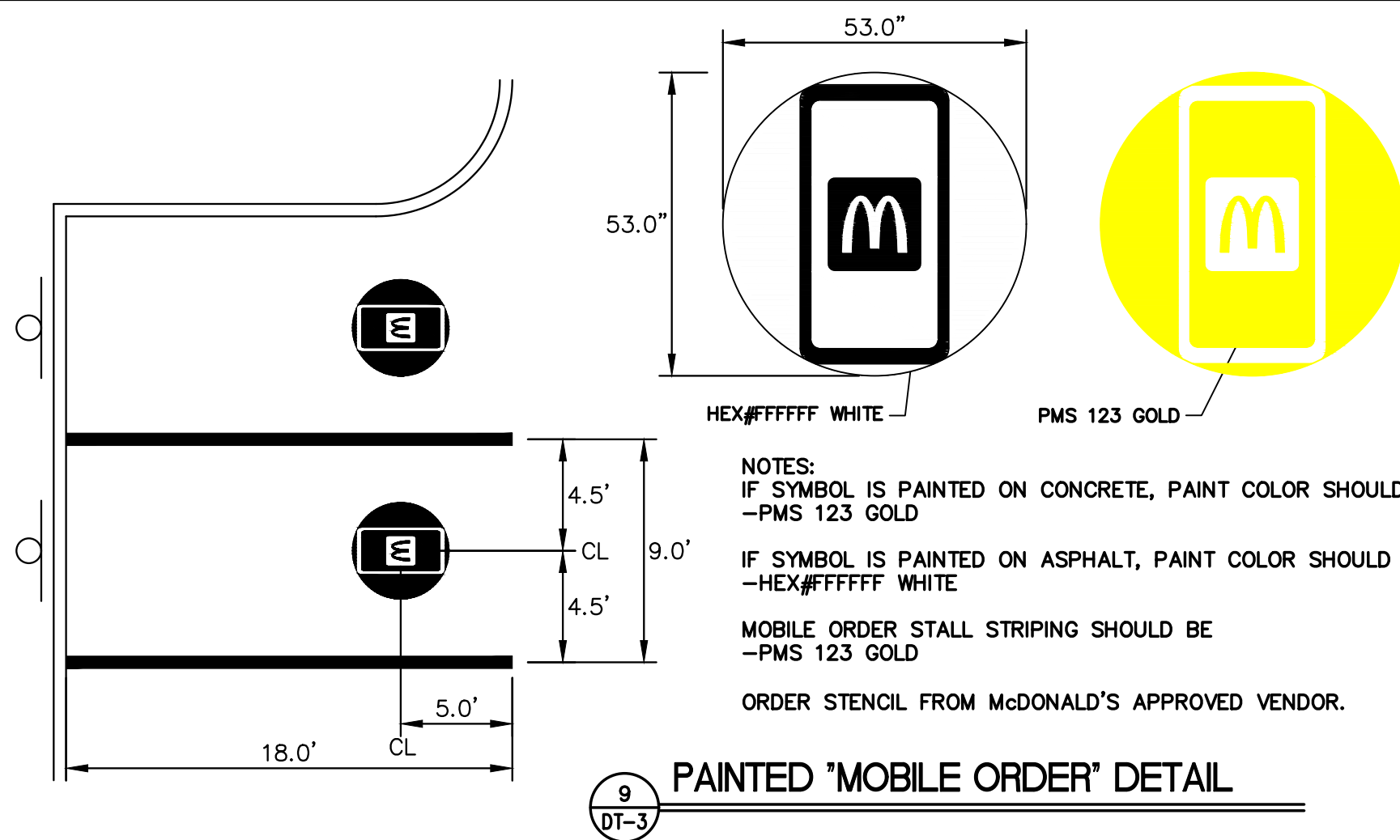
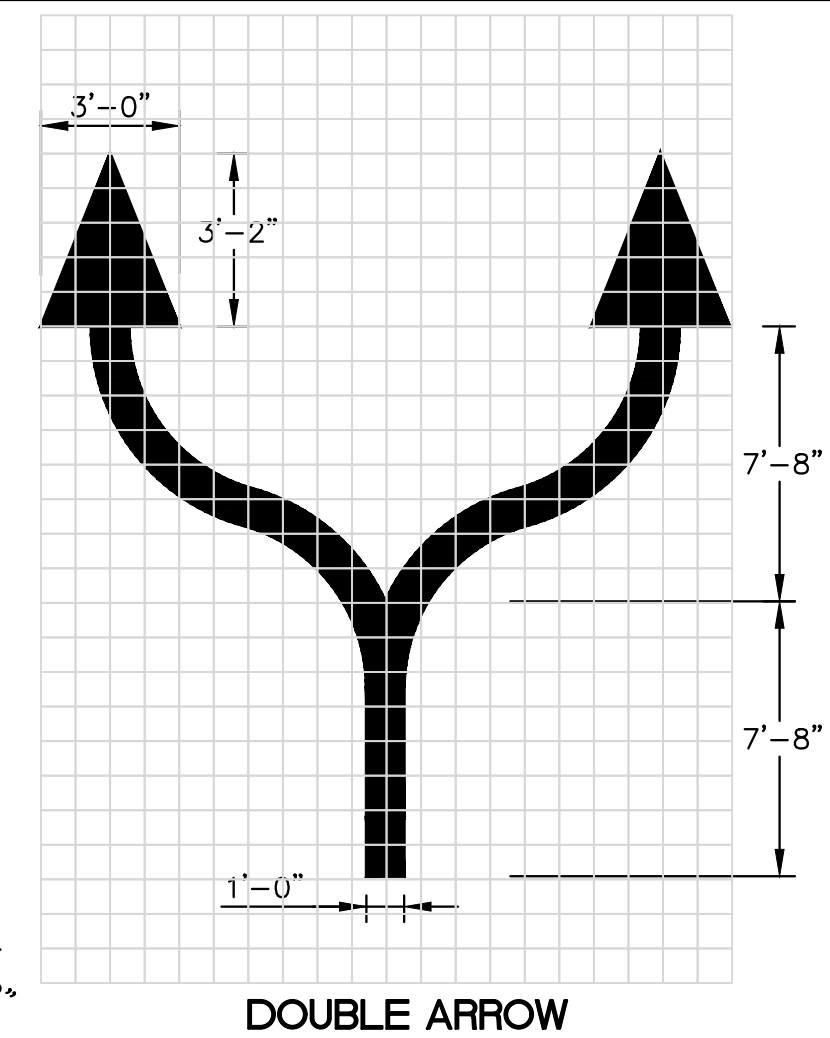
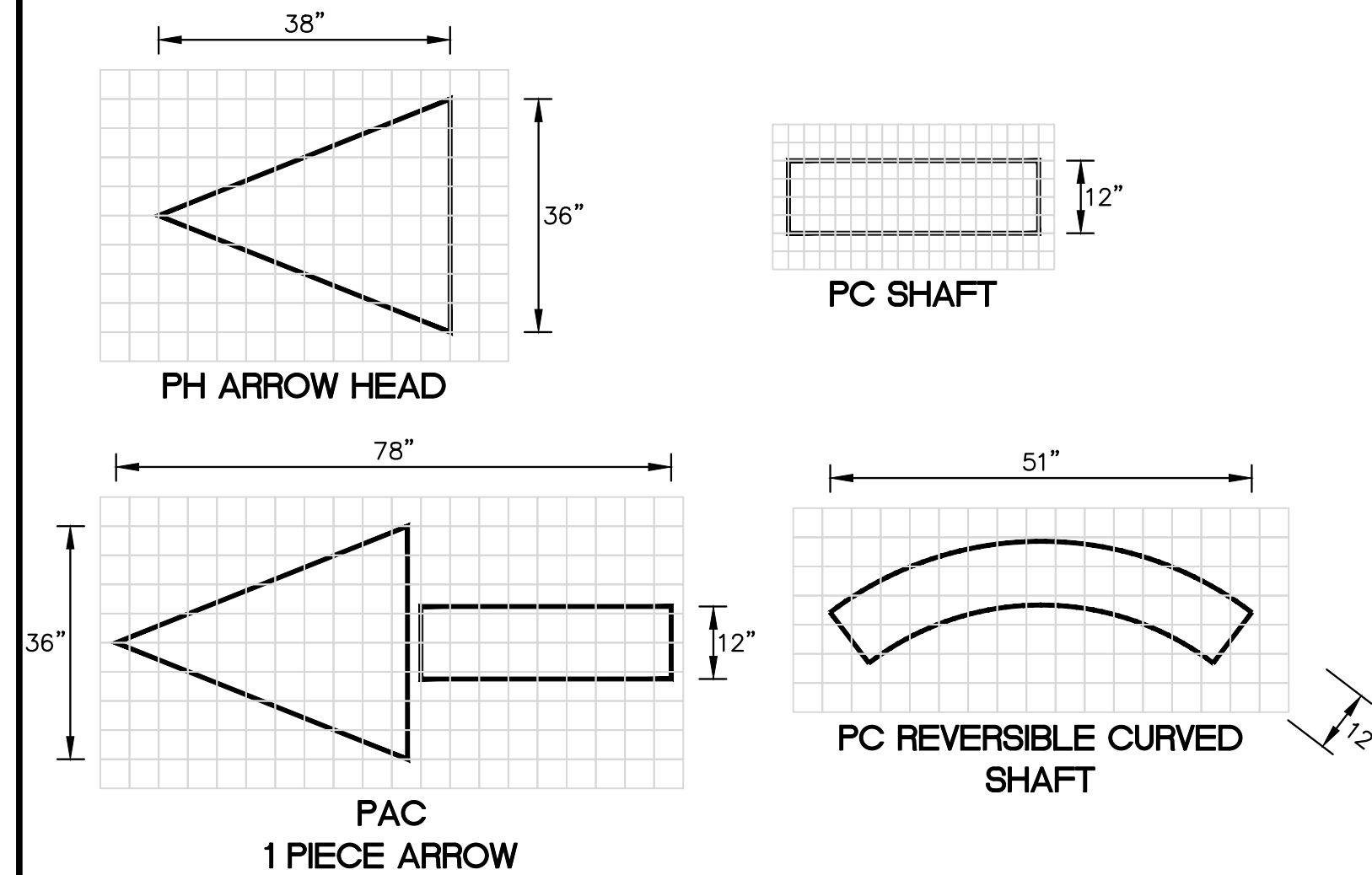
Watermark Engineering Resources
watermark-engineering.com | 2631 Ginger Woods Pkwy | Aurora, IL 60502 | (630) 375-1800

CHECKED BY: J. MILLER
DESIGN BY: D. OLSON
DRAWN BY: JOSH MILLER
DATE: DECEMBER 3, 2024
SCALE: 1" = 5'
PROJECT NO.: 24-001

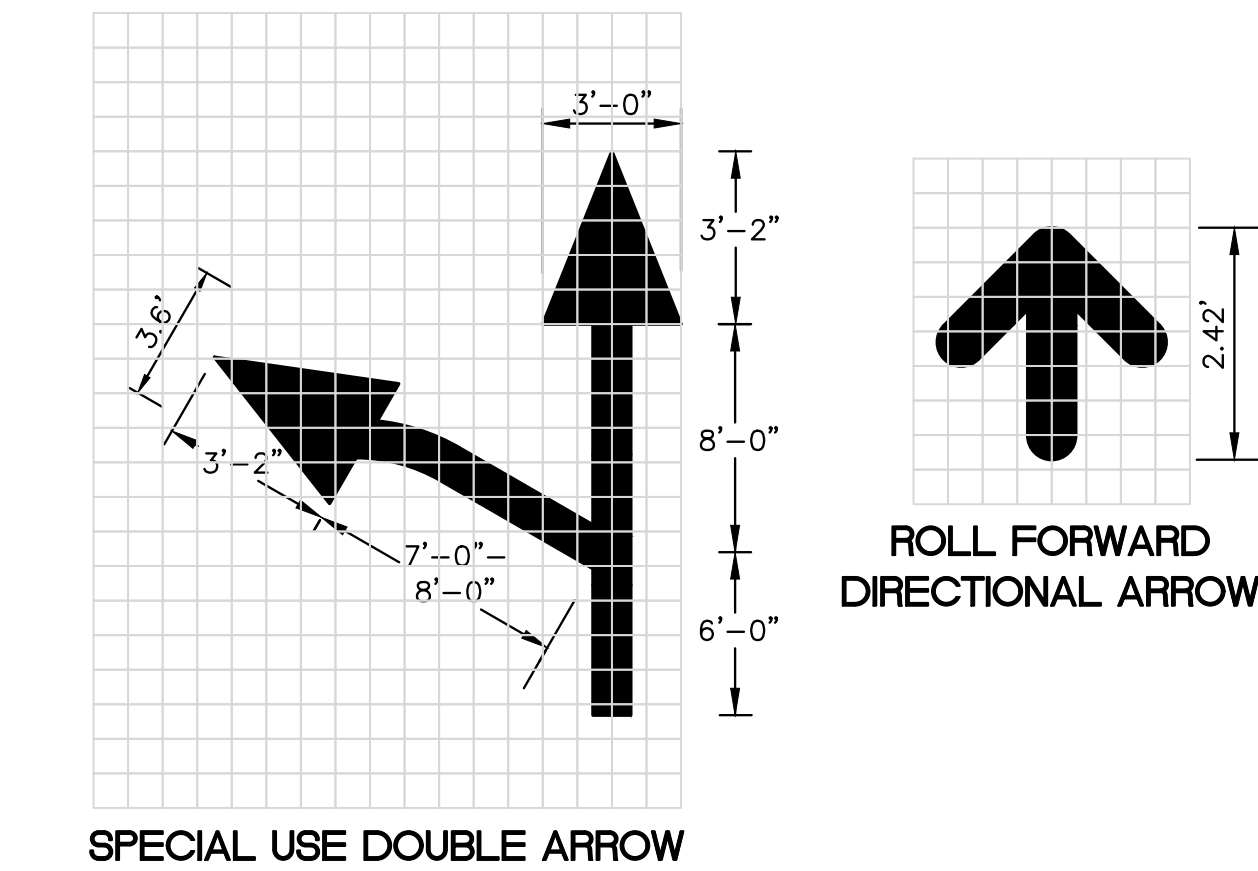
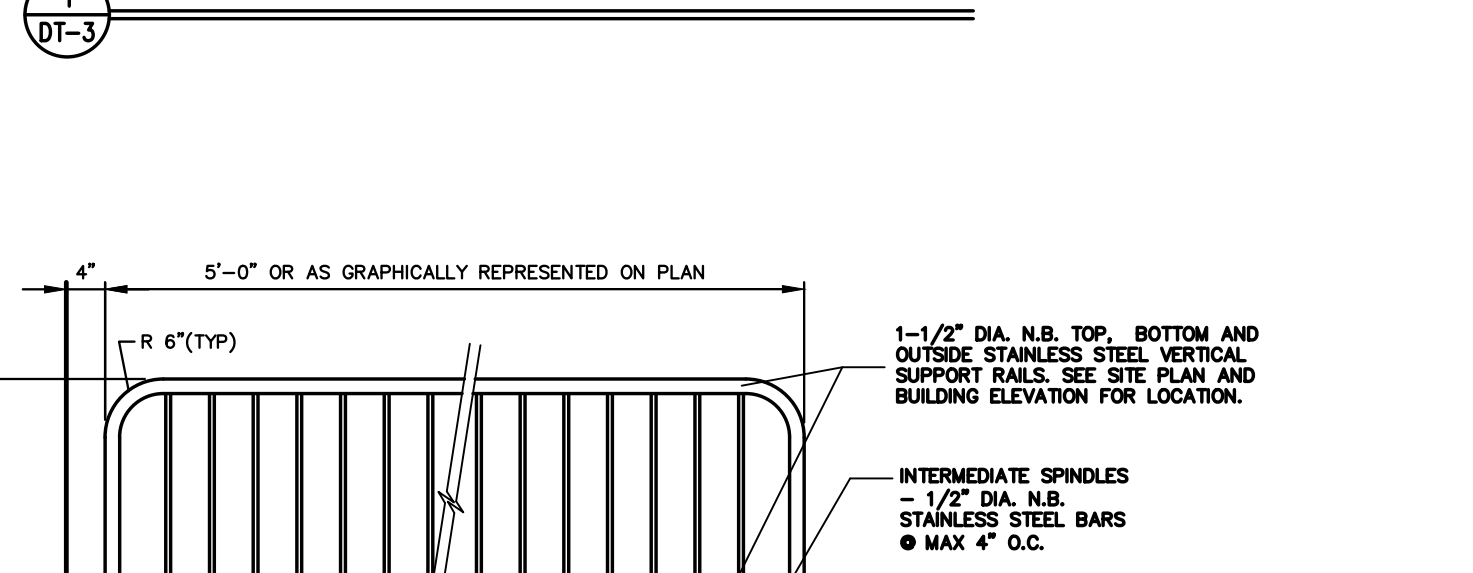
DT-2
LC #48-1082

DRIVE-THRU EQUIPMENT PLAN

PARKING LOT ARROWS (MEETS NO STATE OR FEDERAL DESIGN STANDARDS)

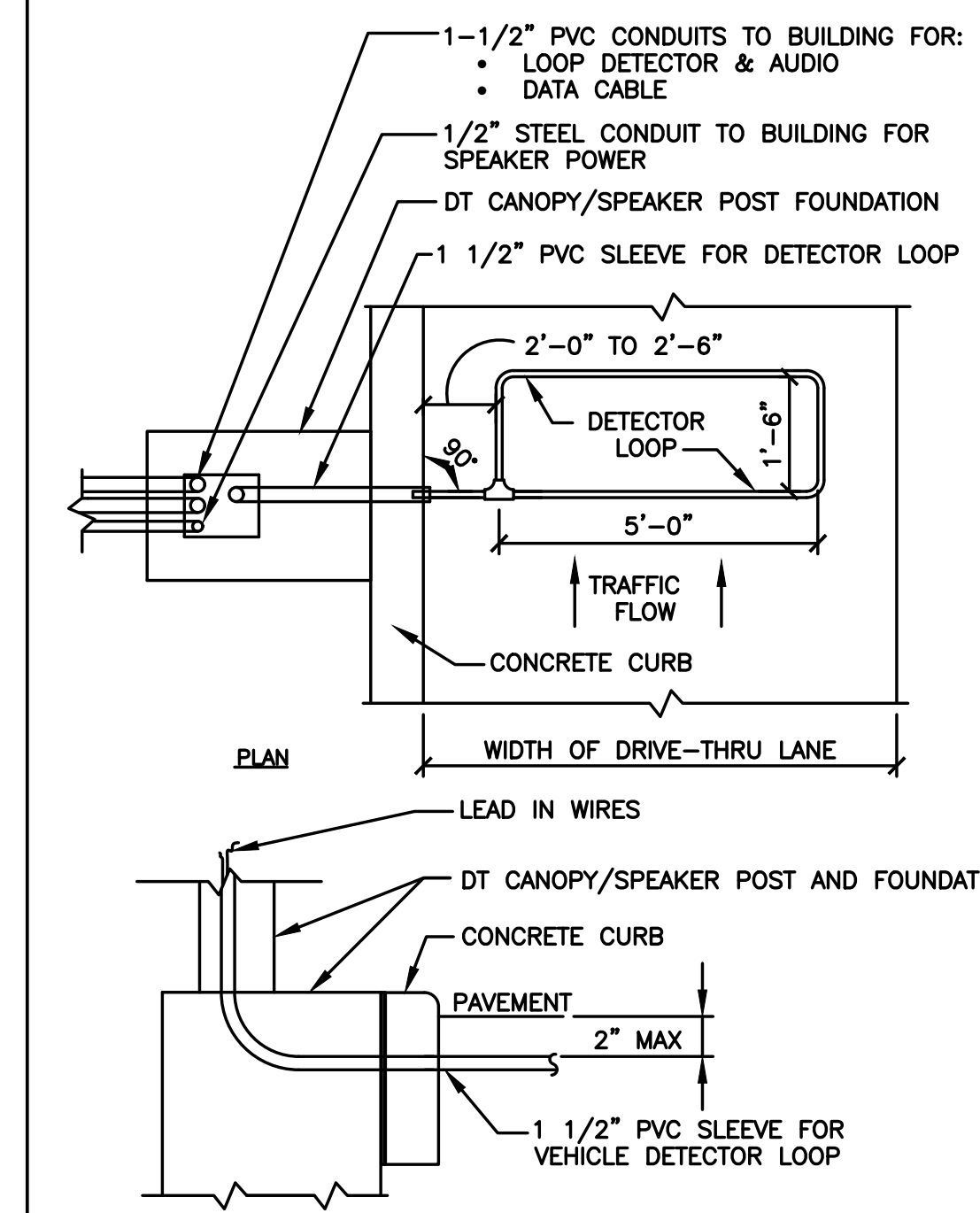
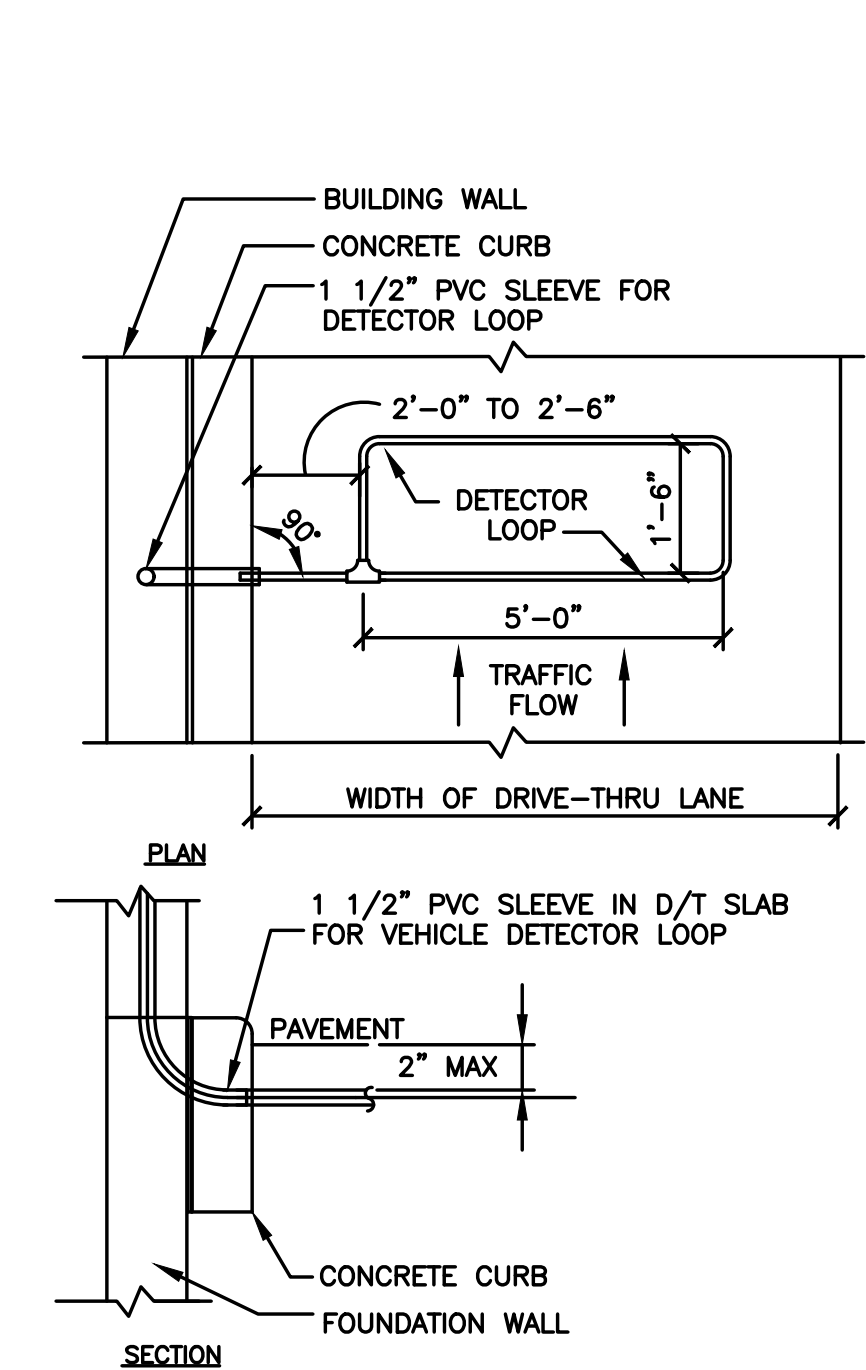


TYPICAL PAVEMENT MARKING DETAILS

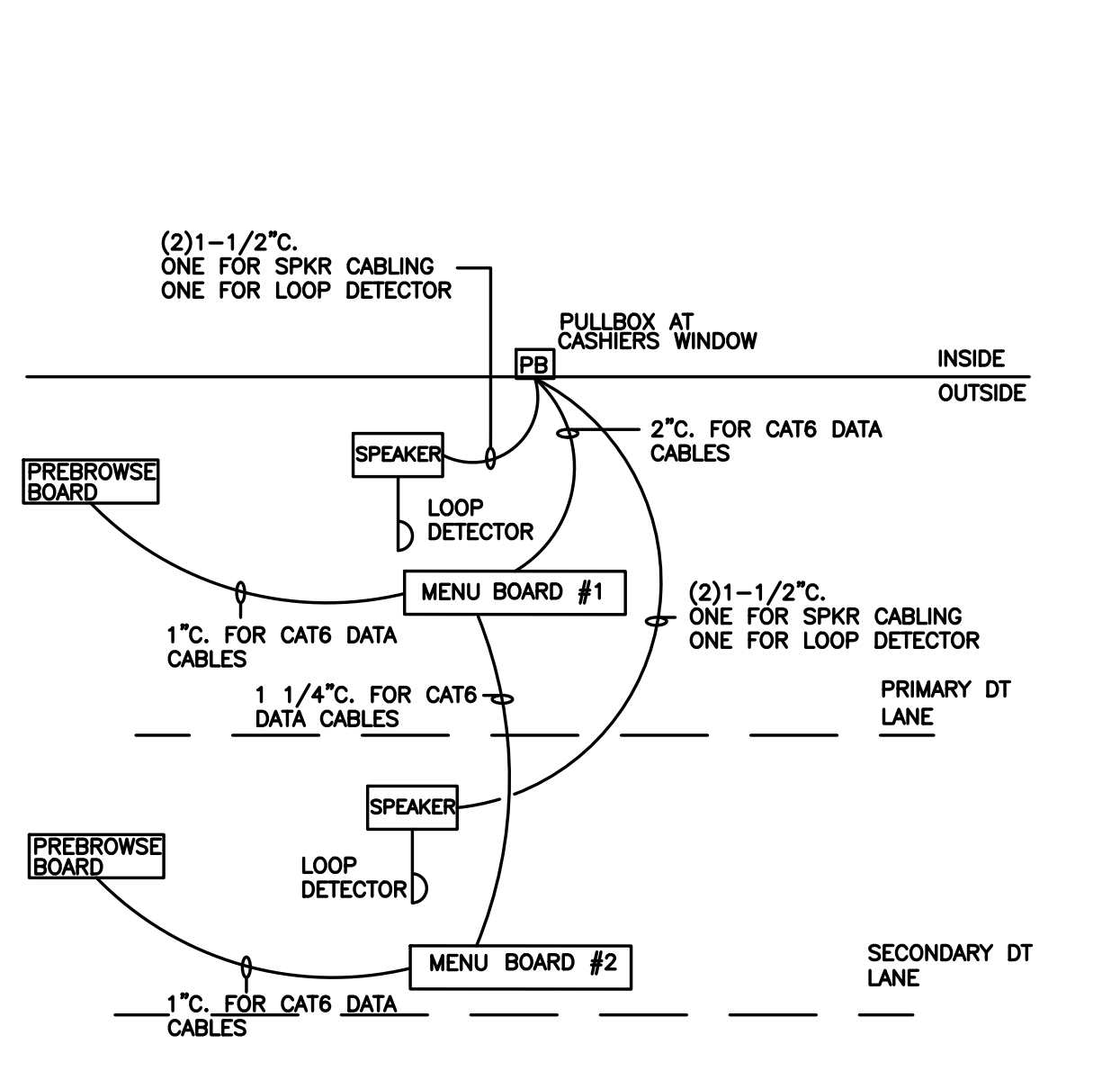
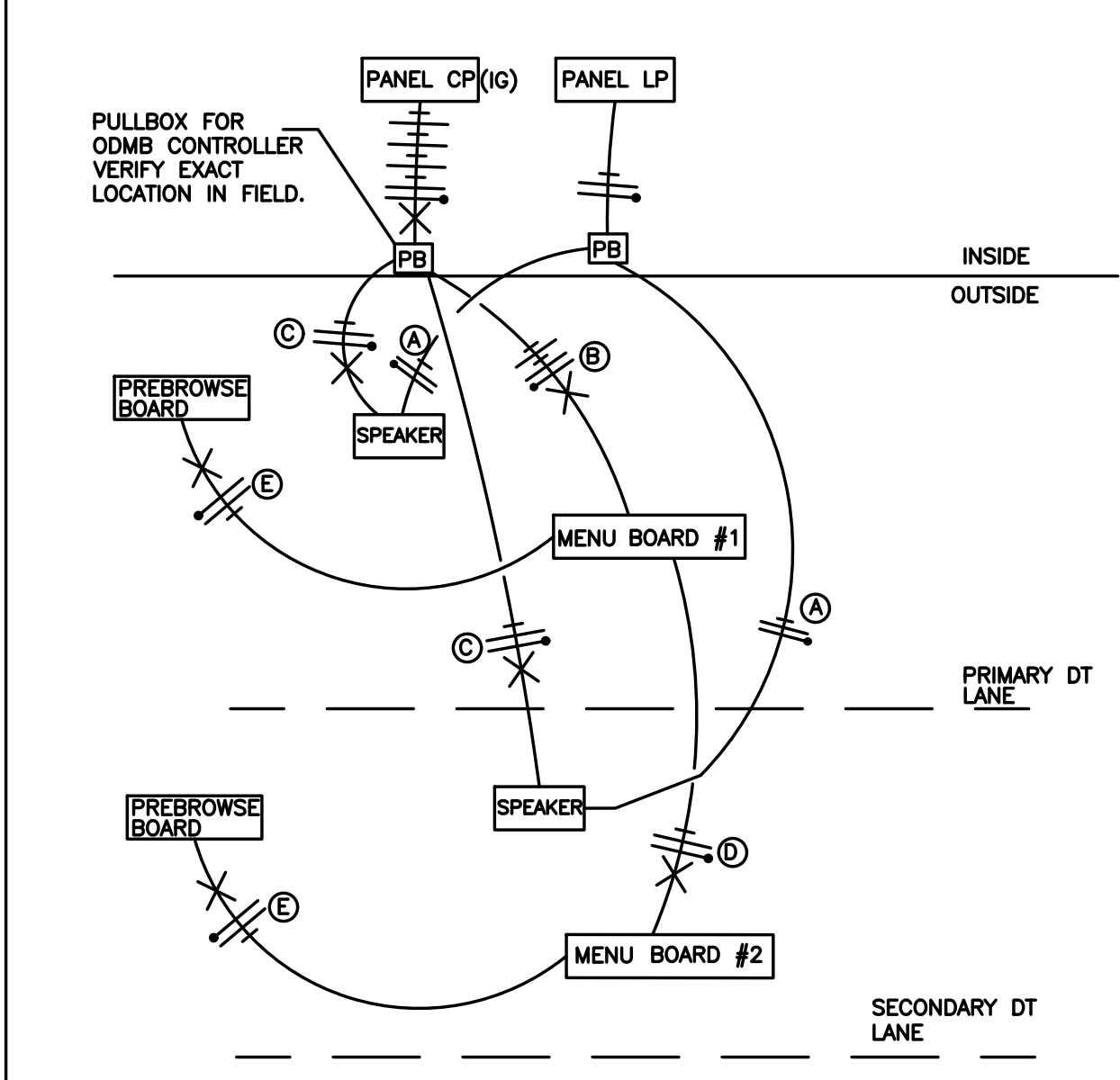
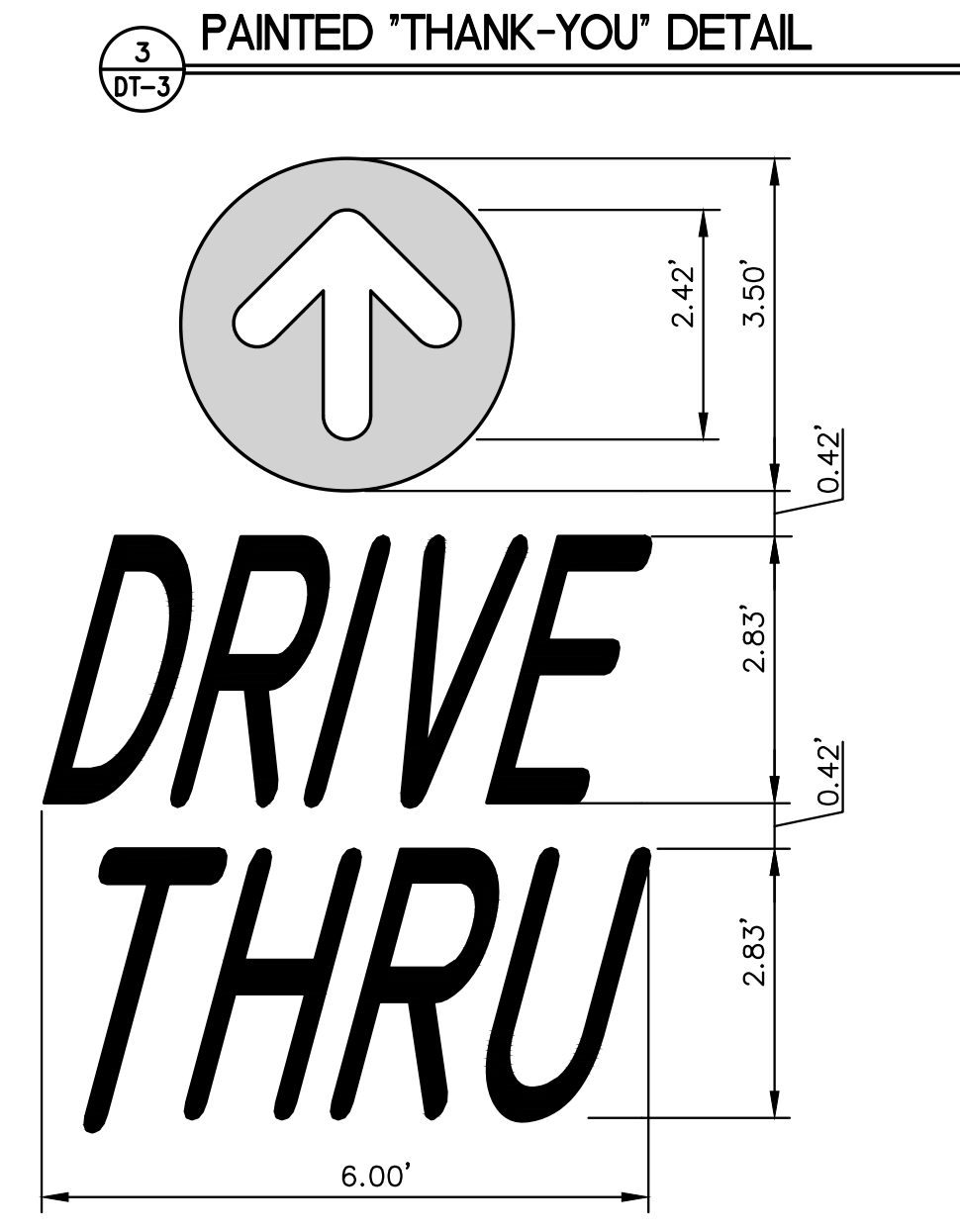
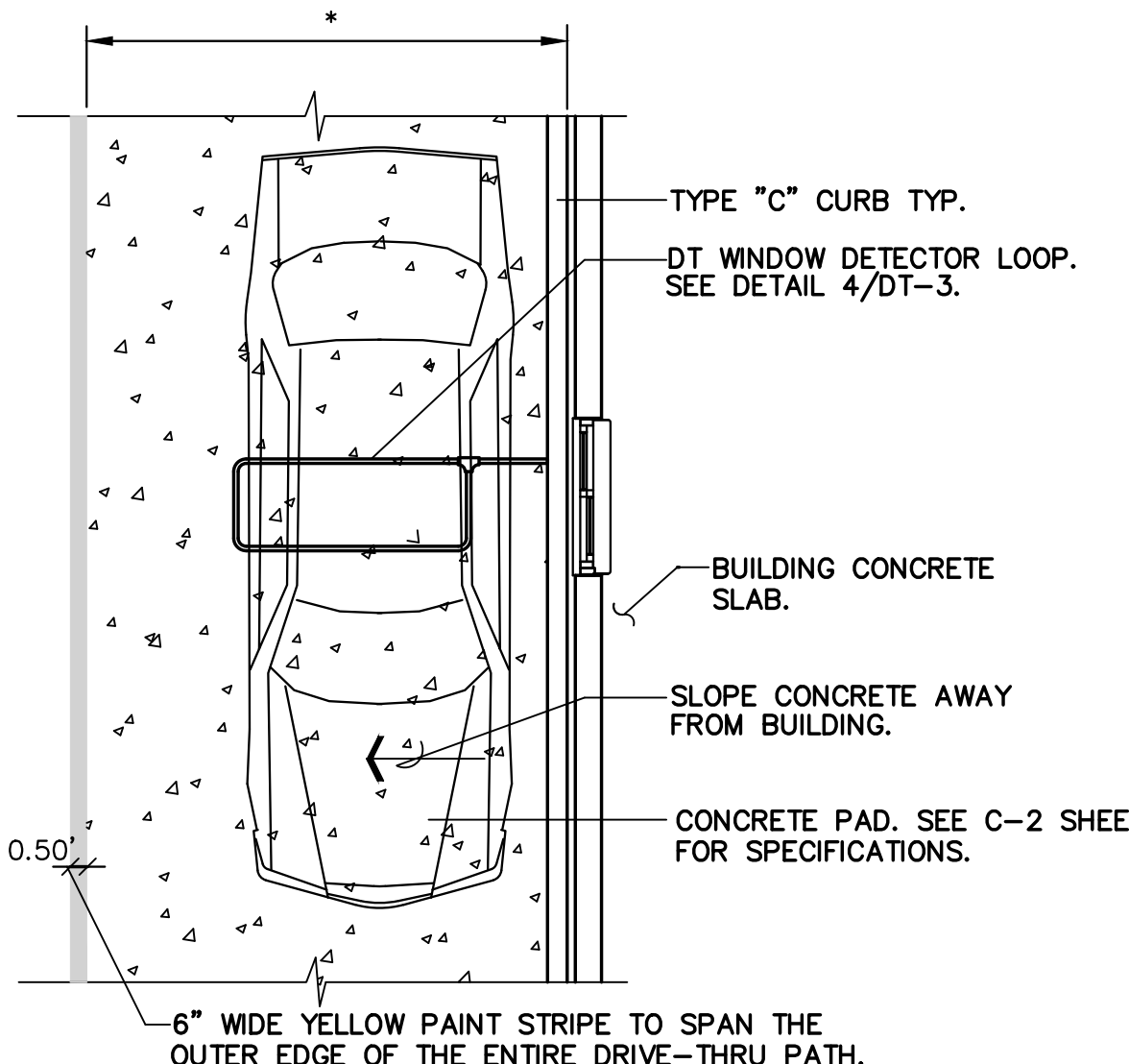


- NOTES**
1. VERIFY CONDUIT SIZES AND LAYOUT WITH DETECTOR LOOP MANUFACTURER.
 2. CENTER VEHICLE DETECTOR LOOP IN DRIVE THRU LANE. INSTALL PER MFR. RECOMMENDATIONS.
 3. NO STEEL (REBAR OR ELECTRICAL WIRE) SHALL BE USED WITHIN 2' OF LOOP.
 4. **DETECTOR LOOP MANUFACTURERS:** DETECTOR LOOPS MAY BE BY ONE OF THE FOLLOWING COMPANIES OR EQUAL.
JM: 1-800-328-0033
HME: 1-800-848-4468
 5. **DETECTOR LOOP MATERIAL:** PVC TUBING 1/2" I.D., 100 PSI LOOP MADE FROM ONE LENGTH OF THIN FOURTEEN GAUGE STRANDED WIRE. LEAD-IN IS PRE-TWISTED AT FACTORY.
 6. **DETECTOR LOOP CONSTRUCTION:** FORMED WITH ONE CONTINUOUS LENGTH OF PVC WITH NO SHARP CORNERS AS DETAILED. WIRE LOOPED, FORMED, & PIGTAILED AS DETAILED.

DETECTOR LOOP DETAILS



1. LOCATION SUBJECT TO ADA MANEUVERING CLEARANCE REQUIREMENTS.
2. THE GUARD RAIL IS INTENDED TO PROVIDE A PHYSICAL BARRIER WHERE INDICATED ON THE PLANS AND IS NOT INTENDED FOR USE AS A HANDRAIL.
3. POSTS TO BE PROVIDED EVERY 3'.
4. GUARDRAILS SHALL BE DESIGNED IN ORDER TO WITHSTAND A LINEAR LOAD OF 50 POUNDS PER LINEAR FOOT AS WELL AS TO RESIST A CONCENTRATED LOAD OF 200 POUNDS IN ACCORDANCE WITH SECTION 6.5.1 OF LATEST EDITION OF ASCE 7.
5. ALTERNATE GUARD RAIL MATERIALS AND COLORS TO BE APPROVED BY McDONALD'S CONSTRUCTION MANAGER.
6. CONTRACTOR TO PROVIDE SHOP DRAWINGS TO McDONALD'S CONSTRUCTION MANAGER FOR REVIEW AND APPROVAL PRIOR TO CONSTRUCTION OF GUARDRAILS.



- NOTE:**
- * VERIFY EXACT CIRCUITS & QUANTITIES OF CIRCUITS WITH PANEL SCHEDULES AND MANUFACTURERS INSTALLATION INSTRUCTIONS.
 - FOR EXISTING LOCATIONS:
 - * VERIFY EXISTING CP PANEL HAS AMPACITY AND SUFFICIENT SPARES/SPACE FOR TWO (2) NEW 20A/1P CIRCUITS. UPGRADE CP PANEL TO 42 CIRCUITS IF NECESSARY.
 - * VERIFY EXISTING PULLBOXES ARE SIZED FOR NEW CONDUIT ROUTING. MODIFY PULLBOXES IF NECESSARY.
- 2#12 & 1#12 GND. TO LP-1 FOR COD CANOPY LIGHTING.
 - 4#12 & 1#12 GND & 1#12 ISOLATED GND. TO CP FOR ISOLATED GROUND POWER TO MENUBOARDS AND MEDIA PLAYERS.
 - 2#12 & 1#12 GND & 1#12 ISOLATED GND. TO CP FOR ISOLATED GROUND POWER TO COD'S. EACH COD SHALL BE ON ITS OWN SEPARATE CIRCUIT.
 - 2#12 & 1#12 GND & 1#12 ISOLATED GND. TO CP FOR ISOLATED GROUND POWER TO MENUBOARDS AND MEDIA PLAYERS.
 - 2#12 & 1#12 GND & 1#12 ISOLATED GND. TO CP FOR ISOLATED GROUND POWER TO FOR PRE-BROWSE BOARDS AND MEDIA PLAYER.

DRIVE-THRU CONCRETE PAD DETAIL

PAINTED "DRIVE-THRU" WITH ARROW DETAIL

DRIVE-THRU DETAILS

McDonald's
110 N. Carpenter St.
Chicago, IL 60607

McDonald's - FITCHBURG, WI
NWC of McKee Road and Fitchrona Road
Fitchburg, Wisconsin

Prepared For: _____

Prepared By: _____

watermark-engineering.com | 2631 Ginger Woods Pkwy | Aurora, IL 60502 | (630) 975-1800

PROJECT NO.: 24-001

SCALE: NONE

DATE: DECEMBER 3, 2024

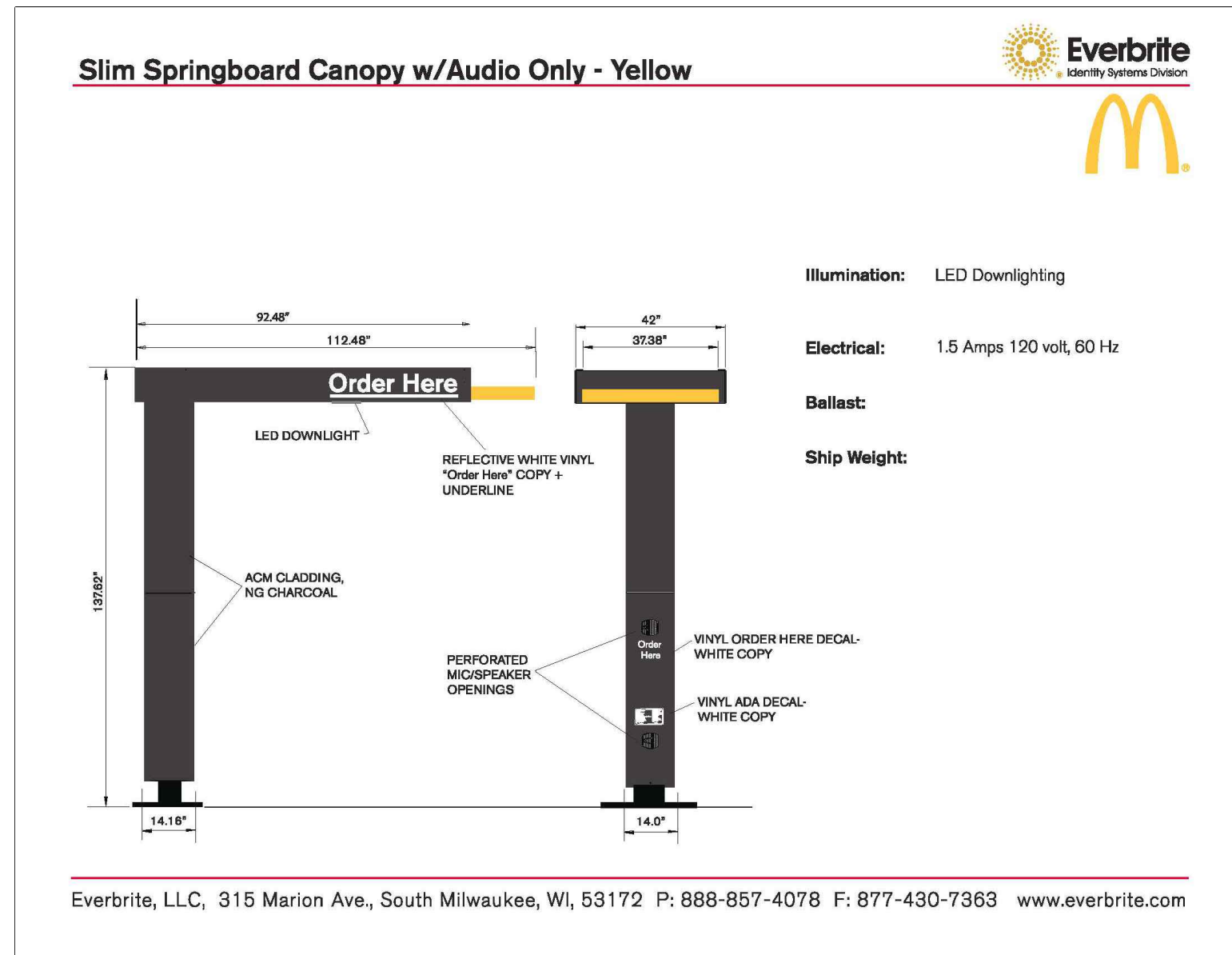
DRAWN BY: JOSH MILLER

DESIGN BY: D. OLSON

CHECKED BY: J. MILLER

DT-3

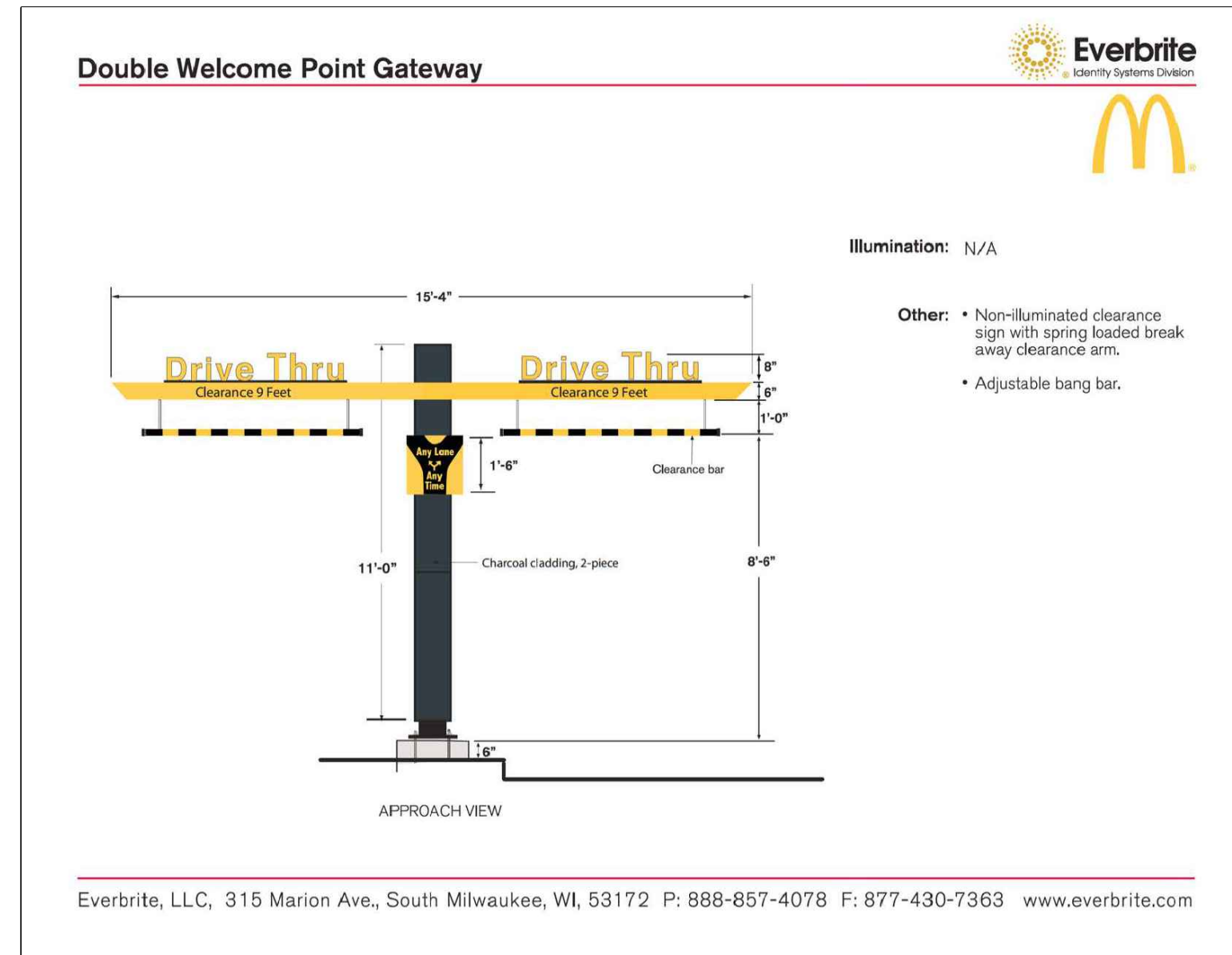
LC #48-1082



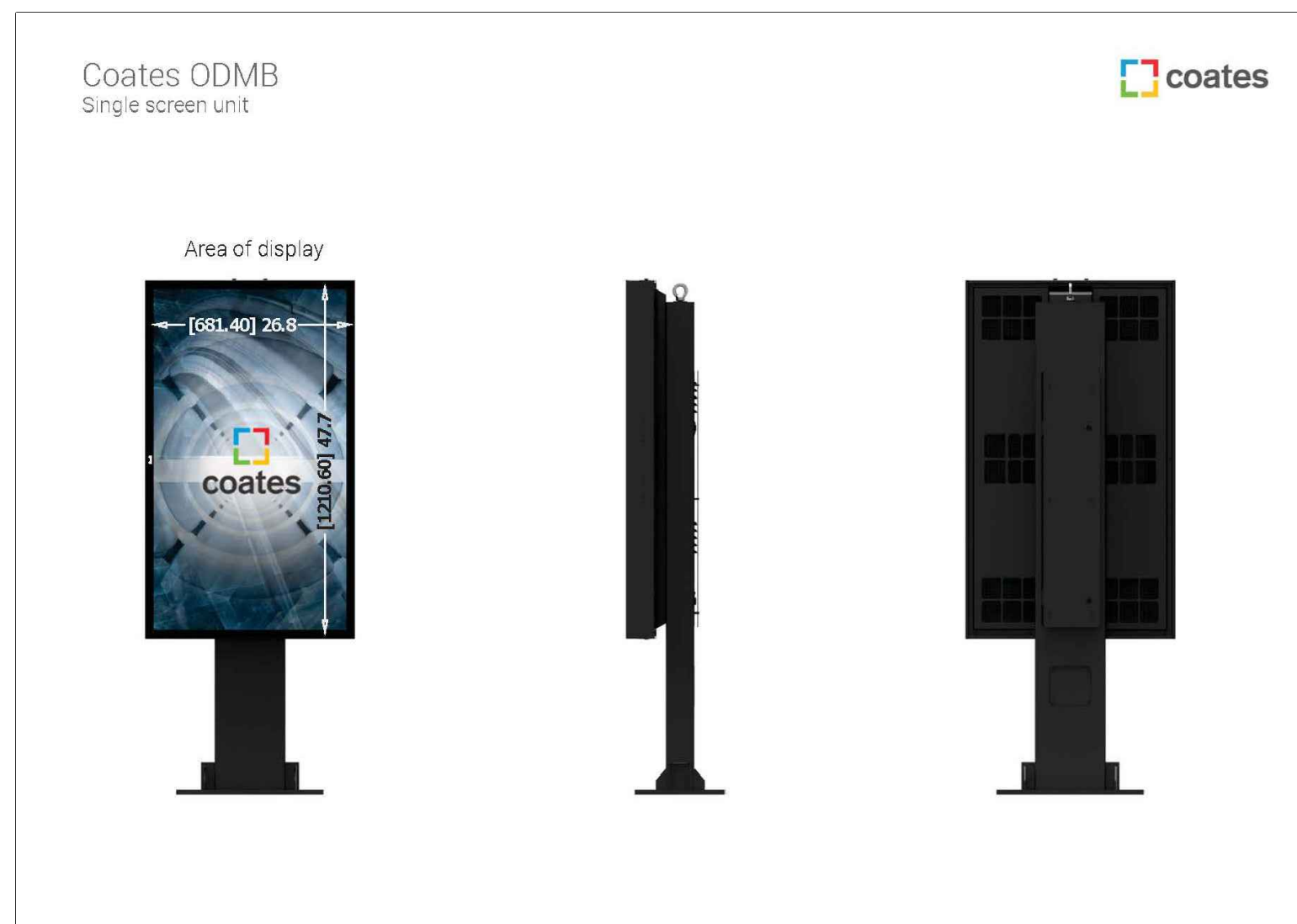
1 DRIVE-THRU SPRINGBOARD CANOPY
DT-4 SEE SHEET SSD-2 FOR FOUNDATION DETAIL



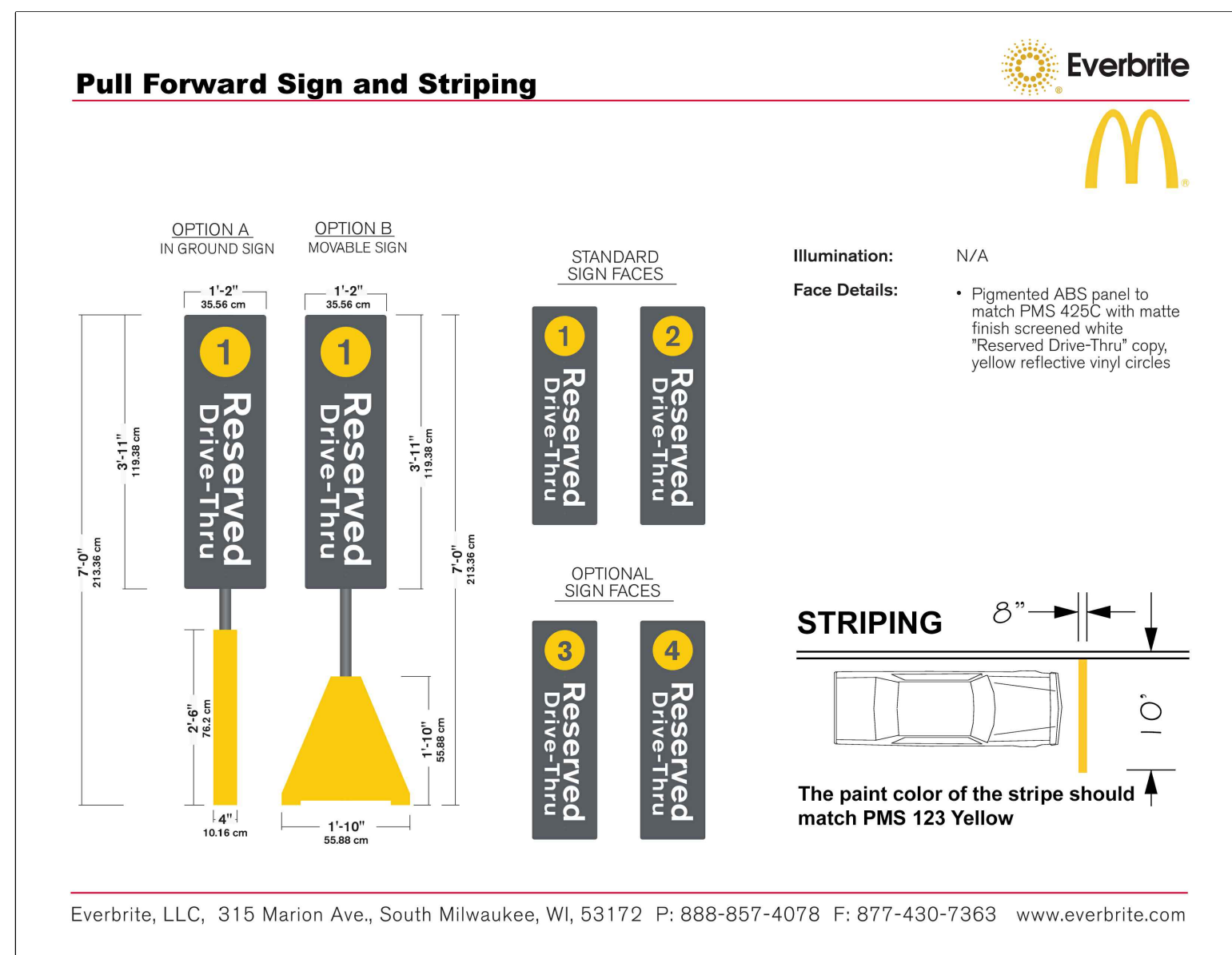
2 ODMB MENU BOARD ELEVATIONS
DT-4 SEE SHEET SSD-1 FOR FOUNDATION DETAIL



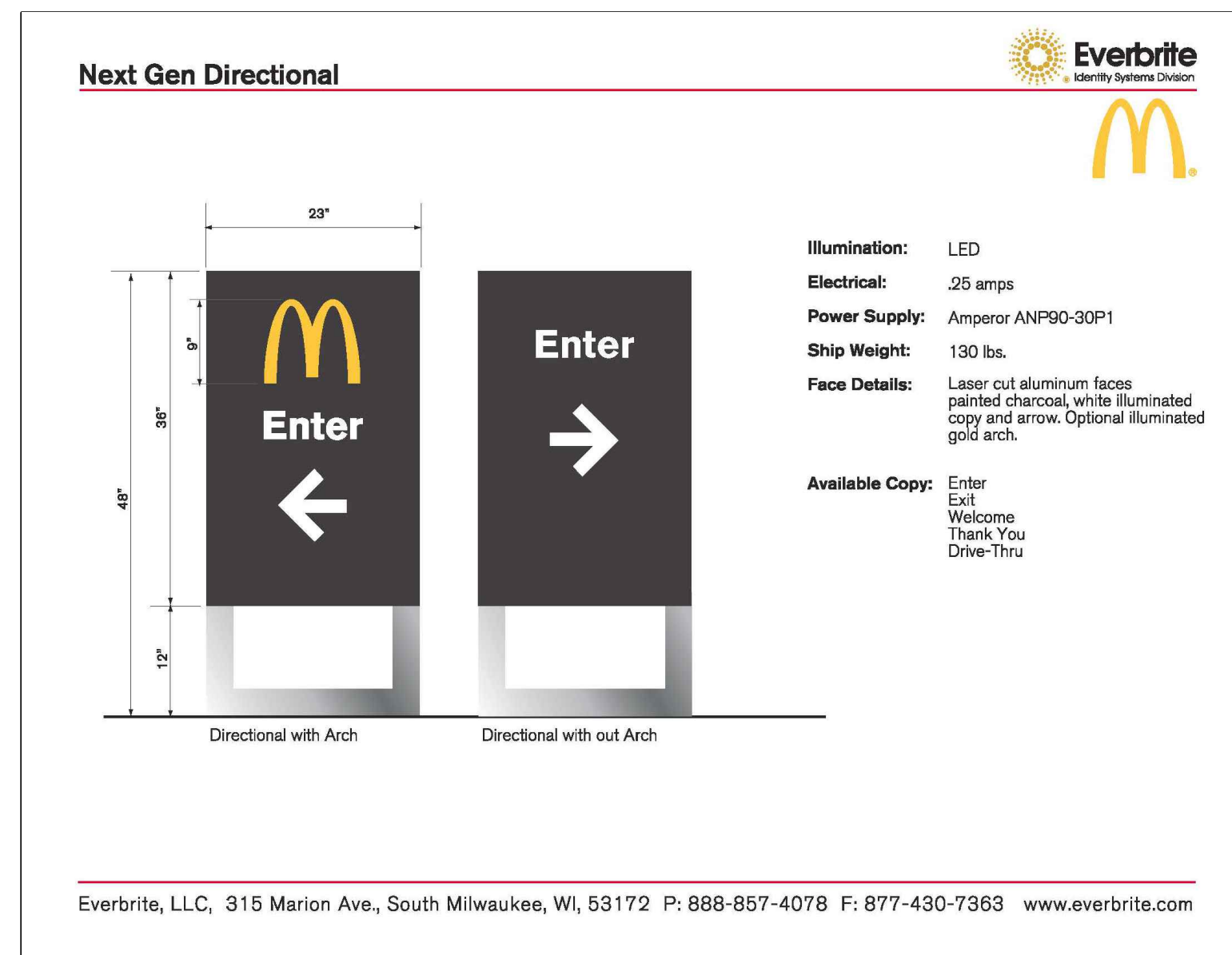
3 DOUBLE WELCOME POINT GATEWAY
DT-4 SEE SHEET SSD-2 FOR FOUNDATION DETAIL



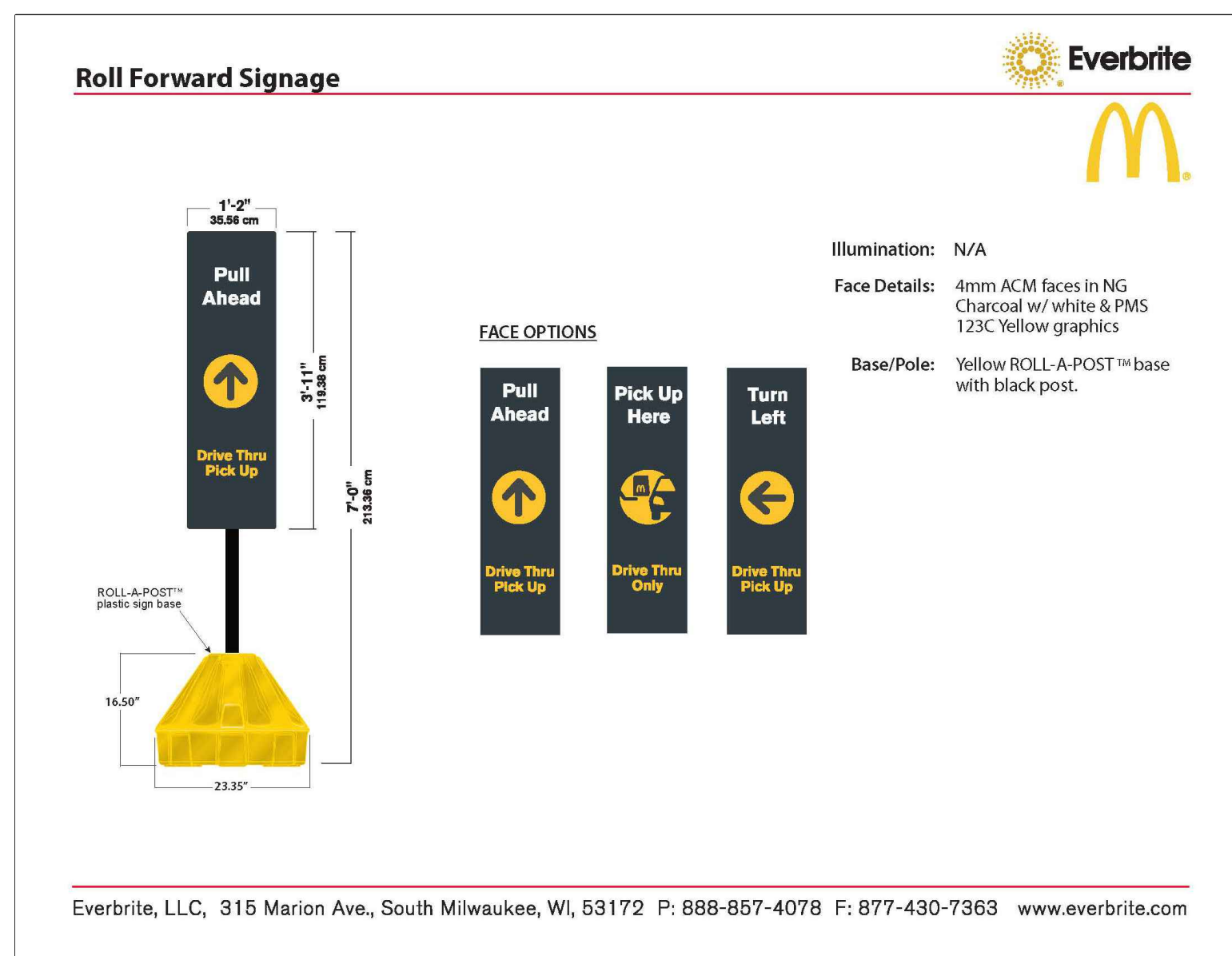
4 DIGITAL PRE-BROWSE BOARD
DT-4 SEE SHEET SSD-1 FOR FOUNDATION DETAIL



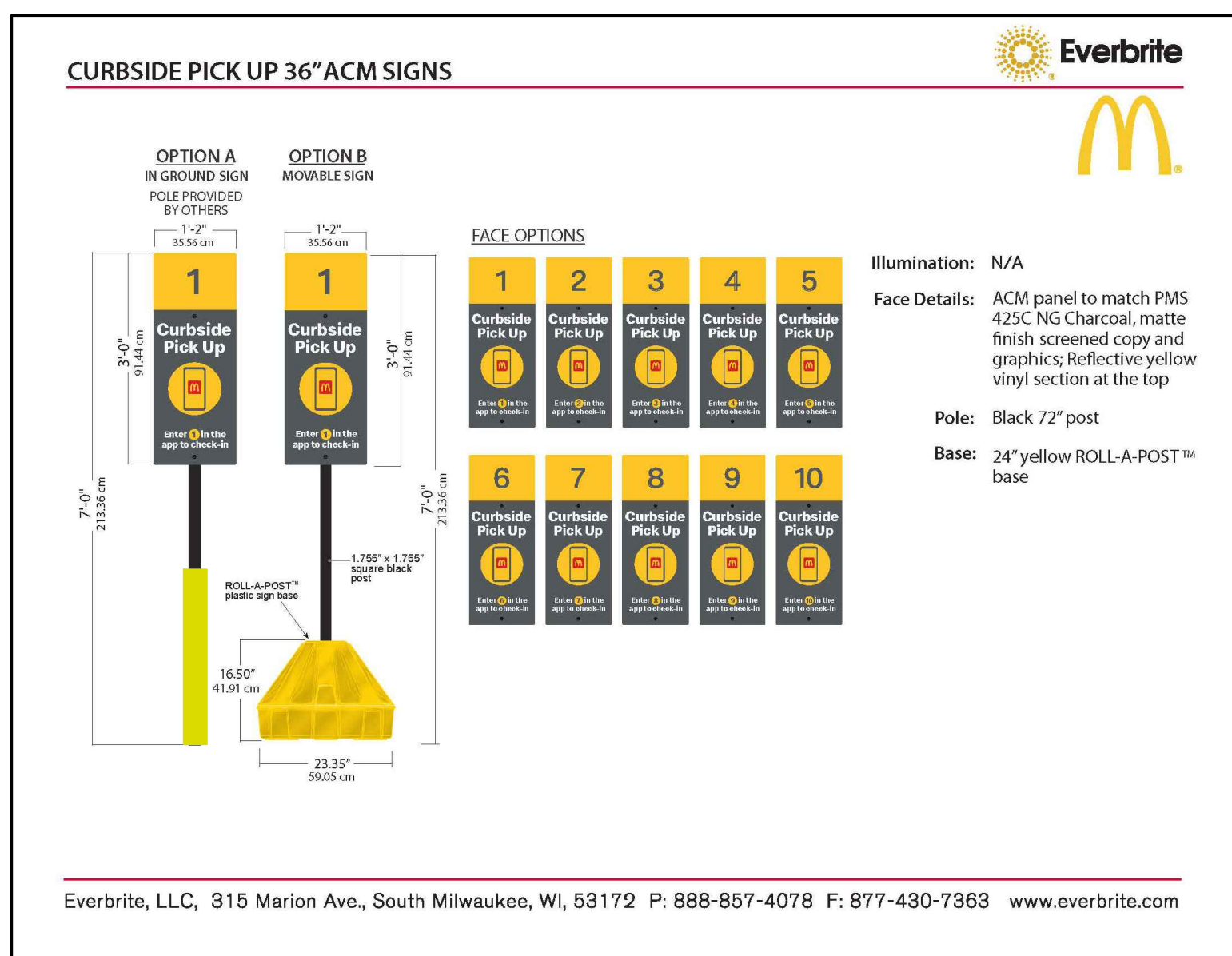
5 PULL FORWARD BOLLARD SIGN AND STRIPING DETAIL
DT-4



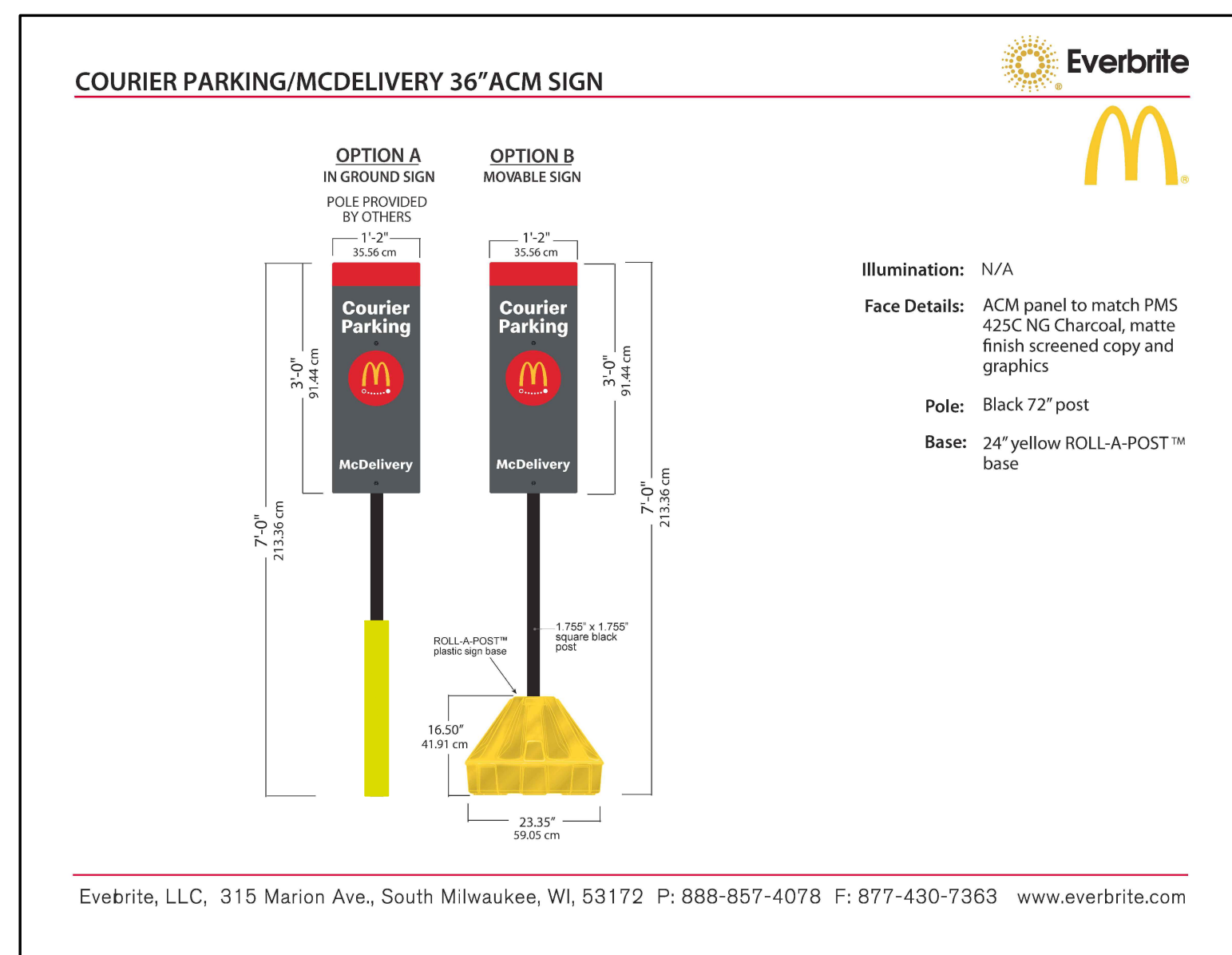
6 DIRECTIONAL SIGN
DT-4 SEE SHEET SSD-3 FOR FOUNDATION DETAIL



7 ROLL FORWARD SIGN AND BOLLARD DETAIL
DT-4



8 READY ON ARRIVAL SIGN AND BOLLARD DETAIL
DT-4



9 MCDelivery SIGN AND BOLLARD DETAIL
DT-4

DRIVE-THRU DETAILS

CHECKED BY: J. MILLER
 DESIGN BY: D. OLSON
 DRAWN BY: JOSH MILLER
 DATE: DECEMBER 5, 2024
 SCALE: NONE
 PROJECT NO.: 24-001

watermark-engineering.com | 2631 Ginger Woods Pkwy | Aurora, IL 60502 | (630) 375-1800

Watermark Engineering Resources

Prepared For: **McDonald's**
 110 N. Carpenter St.
 Chicago, IL 60607
McDonald's - FITCHBURG, WI
 NWC of McKee Road and Fitchrona Road
 Fitchburg, Wisconsin

DT-4
 LC #48-1082

DRIVE-THRU DETAILS



Conditional Use - Owner or Authorized Agent Acknowledgement

** It is highly recommended that an applicant hold at least one neighborhood meeting prior to submitting a CUP application to identify any concerns or issues of surrounding residents.

PLEASE NOTE - Applicants shall be responsible for legal or outside consultant costs incurred by the City. Submissions shall be made at least four (4) weeks prior to desired plan commission meeting.

By signing below, I certify that the information included with this Conditional Use application is true and correct, to the best of my knowledge. Any agent signing below verifies that he/she has the consent of the owner to file the application.

Owner's or Authorized Agent's Signature

1/21/25

Date (DD/MM/YYYY)